

# TOWN OF LANTANA MASTER PLAN



MAY 2023



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## TABLE OF CONTENTS

I. EXECUTIVE SUMMARY .....	4
II. TOUR OF THE PLAN.....	13
WATER TOWER COMMONS	
OCEAN AVENUE - DOWNTOWN	
LANTANA BEACH	
GREYNOLDS CIRCLE	
KMART SITE	
TOWN IMPROVEMENTS	
ARCHITECTURAL CHARACTER	
GATEWAYS	
UPDATING THE CODE	
US-1 REDEVELOPMENT OPPORTUNITIES	
III. KEY RECOMMENDATIONS .....	75
APPENDIX	
A. CREATION OF THE PLAN	
B. BACKGROUND & EXISTING CONDITIONS	
C. ECONOMIC MARKET STUDY EXECUTIVE SUMMARY	

## ABBREVIATIONS & TERMS

CRA	Community Redevelopment Agency
PBC	Palm Beach County
RFP	Request for Proposal
SF	Square Feet
TCRPC	Treasure Coast Regional Planning Council



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## GENESIS OF THE PLAN

Like many communities in South Florida, the Town of Lantana is experiencing increasing pressure for residential and mixed-use development. Incorporated in 1921 and with a current full-time population of approximately 12,100, the Town boasts a healthy mix of residential, commercial and recreational elements. Despite being mostly built out, Lantana is seeing a second and third generation of development and redevelopment interest.

The Town has concerns that the lack of a cohesive vision, coupled with antiquated zoning regulations that do not meet current redevelopment demands, could hamper the Town’s ability to guide future growth in a manner suitable to the community. What are appropriate building heights, building scale and positioning for new development in the Town? What are strategies to accommodate future growth yet maintain the “Seaside Fishing Village” character? These questions and others were to be explored through the master planning process.



In January 2022, the Town of Lantana (Town) entered into an agreement with the Treasure Coast Regional Planning Council (TCRPC) to conduct a public design charrette and develop a master plan establishing a vision for future redevelopment, identifying pressing community concerns, considering needed multi-modal improvements, and providing a visual blueprint that expresses the desires of the residents going forward.

In addition, the Town prudently sought to develop a real estate market study to understand the current economic conditions and environment of the town and quantify realistic expectations for future supportable growth in the residential, retail/commercial, industrial, and hospitality markets.

## PUBLIC DESIGN CHARRETTE

On Saturday, July 9, 2022, the TCRPC team kicked off a five-day public design charrette held at the Town Council chambers. The charrette culminated two months of pre-charrette stakeholder interviews and meetings with the Charrette Host Committee to organize the logistics of the effort and explore community concerns. On Monday, July 25, 2022 a Work-in-Progress presentation was given to illustrate work to date and solicit further public input. Another Work-in Progress presentation was provided on Monday, September 19, 2022 to ensure there was ample opportunity for public feedback on the direction of the plan.

During the Saturday workshop the community was provided maps of the entire town and asked to identify areas of concern and needing attention. Working with the TCRPC team, the groups were able to hone these community challenges into identifiable, actionable items. The prioritized focus areas, discussed in detail in this report, included Water Tower Commons, Ocean Avenue, Lantana Beach, US-1 infill redevelopment and connectivity improvements, the zoning code, Town Hall, parking, and the Winn Dixie-Kmart site.

This Master Plan report illustrates the development of the various design and regulatory recommendations made to the Town of Lantana based upon input from the public provided throughout the charrette and public presentation process.

The image to the right was taken during the public design session on Saturday, July 9th in the Council Chambers.



## ECONOMIC MARKET STUDY

TCRPC worked with the nationally recognized economists WTL+a Real Estate and Economic Advisors and Retail & Development Strategies, LLC. to develop a detailed market study for the Town of Lantana. The market study explored the current economic conditions of the Town but perhaps most importantly, established the reasonably supportable future growth for the Town. The following are excerpts from the Executive Summary of the market study report which can be found, in its entirety, in Appendix C.

WTL+a, a national real estate and economic development consulting firm in Washington, DC and Massachusetts, with significant project experience throughout Florida, was retained in June 2022 by the Treasure Coast Regional Planning Council (TCRPC) to prepare a real estate market analysis (the “market study”) as part of a town-wide Master Plan for the Town of Lantana in Palm Beach County. WTL+a was assisted in this effort by Retail & Development Strategies LLC of Arlington, VA.

The study is intended to provide Town officials with economic and real estate market data and an evaluation of development potentials to guide public decisions regarding planning and zoning, land uses and entitlements and possible regulatory or other types of incentives to enhance the long-term growth and economic development of the Town.

Lantana is a small municipality in Palm Beach County comprising 2.97 square miles and located between Boynton Beach and Lake Worth. Municipal boundaries generally include Hypoluxo Road on the south; I-95 on the west; the Intracoastal Waterway on the east; and the border of the City of Lake Worth Beach on the north. In terms of economic anchors, between 1950 and 2012 Lantana was home to the A.G. Holley Hospital, a 162,000 sq. ft. state-run sanitarium for tuberculosis patients, which the state closed in 2012. Between 1971 and 2000, Lantana was also the location of the headquarters of the National Enquirer. According to the U.S. Census, the Town’s population increased from 10,423 residents in 2010 to 11,504 residents in 2020, and 11,579 in the 2021 state estimate.

The four potential land uses analyzed in the real estate market study included:

- Multi-family residential (for-sale and/or rental)
- Workplace (including both professional office and light/general industrial)
- Community retail (stores, food & beverage, consumer services, etc.)
- Visitor-supporting hotel/lodging



## HOUSING: CHARACTERISTICS & MARKET POTENTIALS

Despite market uncertainties in the Town's commercial sectors, its housing market is strong as evidenced by the construction of more than 1,000 new units since 2018, including Water Tower Commons and Aura Seaside. While the number of owner-occupants decreased from 46% in 2010 to 42% in 2021, average values of owner-occupied housing units increased to \$499,400 in 2021. Moreover, the Town's 'True Vacancy' rate (i.e., excludes seasonal, migrant and units for-sale) has decreased—from 6.7% in 2010 to 4.6% in 2019.

In effect, Lantana's housing market is stabilized (the real estate industry considers a 5% vacancy rate to reflect stabilized market conditions) and investment interest from national housing developers and investors is increasing as exemplified by Water Tower Commons and the proposed redevelopment of the Kmart site.

The Economic Market Study suggests that if Lantana continues to grow at a sustained compound annual rate of 1.44% per year—as forecast by ESRI Business Analyst—over the next 10 years, this growth could expect to generate more than 1,700 new residents and **sufficient demand to support up to 950 new housing units town-wide by 2030**. This includes an allocation to the known projects (proposed, approved and/or under construction). After accounting for these projects, there remains market support for approximately **340 “unallocated” units town-wide over the next 10 years**.

## WORKPLACE: CHARACTERISTICS & MARKET POTENTIALS

Lantana has a very limited supply of office space, with an inventory of only 194,000 sq. ft. in 22 buildings. It is considered a tertiary office submarket in Palm Beach County, with only 0.33% of the County's office space. Other metrics indicative of a tertiary location include:

- Limited new construction—only 14,000 sq. ft. of new office space has been built in the last 15 years;
- Net absorption is negligible—averaging only 1,070 sq. ft. per year between 2007–2021 and 2,600 sq. ft. per year over the past five years;
- Low office rents—in the range of \$19 to \$21 per sq. ft.—are not considered investment-grade; and
- Vacancy rates of 2.8% through summer 2022 are considered stabilized (the real estate industry considers a 5% vacancy rate to reflect stabilized market conditions).



**Market potentials for multi-tenant/speculative office space should comprise no more than 20,000 to 30,000 sq. ft. of space** oriented to professional services tenancies and located above street-level retail. The capacity to support any additional office space beyond this amount will be determined by growth in specific business markets and office-using sectors, transition of home-based businesses into leased space, rental/occupancy costs compared to competing nearby locations, etc.

The town's inventory of another workplace sector—industrial space—totals 317,500 sq. ft. in 27 buildings (1.3% of the County). Only 16,200 sq. ft. of new industrial space has been delivered since 2007. Notably, **industrial vacancy rates in Lantana are very low—ranging from 0% to 1%—even with a reported loss of 65,900 sq. ft. of occupied industrial space (i.e., negative net absorption)**. As a result, limited supply and low vacancies are generating above-market rents ranging from \$10 to \$18 per sq. ft. for industrial/flex space.

## COMMUNITY-SERVING/RETAIL: CHARACTERISTICS & MARKET POTENTIALS

Based on the windshield survey conducted by RDS LLC, the Town of Lantana contains approximately 1.1 million sq. ft. of retail space in 283 storefronts. The largest merchandise categories include Specialty Retail (like Costco), Food & Beverage/Groceries and Consumer Services, which comprise approximately 25% each. The empty Kmart store accounts for the lion's share of Lantana's vacant retail space. This inventory suggests there are 90+ sq. ft. of retail space for every resident of Lantana—significantly above both the County and national averages.

According to CoStar, Inc., net absorption of retail space averaged 5,400 sq. ft. per year over the past 15 years and only 4,500 sq. ft. per year over the past five years. Since 2007, only 86,200 sq. ft. of new retail space was built in Lantana.

Opportunities for additional/new retail space in Lantana in the future are predicated on the following key assumptions/objectives:

- Increased market capture is a more achievable objective, without any substantial expansion in space;
- There are only a limited number of redevelopment sites/parcels, but these can be better integrated with context;
- A critical objective for the Town is to create focal public space(s) with adjacent retail businesses that generate pedestrian activity and a critical mass of retail businesses;
- Participation among fragmented/multiple property owners will be critical for key sites, particularly in the downtown/Ocean Avenue corridor; and
- Consider updating building codes and parking standards, which are challenging and likely resulting in limited new retail development.

2030 Retail Market Potentials: **20,000 to 25,000 SF With 7,000–12,000 SF of General Retail & 12,000–14,000 SF of Food & Beverage.**



## VISITOR-SERVING/HOTEL: CHARACTERISTICS & MARKET POTENTIALS

To understand hotel market performance and opportunities for hotel development in Lantana, WTL+a obtained hotel performance data from STR Global for 13 selected properties containing 1,600 rooms and located primarily on nearby commercial corridors and I-95 interchanges in Lantana/Lake Worth/Boynton Beach. Of this competitive supply, Lantana has 387 hotel rooms in six properties. It is critical to understand market conditions given the enormity of the impacts on the hotel and hospitality industries from the COVID pandemic as well as the time required for recovery.

The COVID pandemic which struck in March 2020 significantly impacted the hotel/hospitality industry worldwide. Among the competitive properties, **monthly occupancies dropped precipitously—from 87.6% in February 2020 to 24.2% in April 2020**. Overall occupancies among these 13 properties averaged 49.2% at year-end 2020, and ongoing recovery in 2021 yielded a year-end occupancy of 64.9%. During the first five months of 2022, occupancies had returned to pre-COVID performance in the range of 67% to 87%. Notably, sustained improvements in performance metrics also occurred in average daily rates (which increased by 28% between 2020 and 2021) and revenue per available room (“RevPAR”) (which jumped by 69%). Further improvement among these metrics continued into 2022.

This analysis reveals **market support for 200 to 225 new hotel rooms in the Lantana/Lake Worth/Boynton Beach trade area**. If peak growth in roomnight demand (which occurred between 2014 and 2018 among the competitive set) continues, it is sufficient to support up to 225 additional hotel rooms in the surrounding area over the next 10 years. The key question is how much of this demand (in rooms) *could* Lantana capture? This will be predicated on parcel location and attributes such as water views, highway access/frontage, visibility, etc.



**Table 1: Lantana Retail Inventory, 2022**

	In SF (Rounded)	% of Total	No. of Storefronts
Specialty Retail (Including Costco)	302,400	27%	41
Food & Beverage, Groceries, Liquor	257,000	23%	61
Consumer Service Businesses	253,300	22%	73
Finance, Insurance and Real Estate	32,600	3%	23
Professional Offices (in Storefronts)	41,400	4%	19
Automotive/Marine Uses	103,800	9%	26
Vacant (Including K Mart)	146,400	13%	40
<b>Total Estimated Retail Space</b>	<b>1,136,900</b>	<b>100%</b>	<b>283</b>

Using potential sales from all consumer sources, new/incremental retail demand is likely to be generated primarily by new residents (Water Tower Commons, Kmart redevelopment, etc.), with pass-through traffic and visitors comprising a smaller share of market support. Other opportunities for new retail are possible as replacements for existing underperforming, undercapitalized and marginal businesses or in existing vacant space.

Retail Space Per Capita (In SF)	ESRI	State
<b>Town of Lantana</b>		
- 2021 Population	12,439	11,579
- SF Per Capita	<b>91</b>	<b>98</b>
<b>Palm Beach County</b>	<b>54</b>	
<b>U.S./National Average</b>	<b>24</b>	
	<b>to</b>	
	<b>49</b>	

Source: RDS; WTL+a, July 2022.



Table 2: Summary of Market Potentials

Land Use	Gross Demand	Net/Unallocated Demand	Notes
Market-rate Housing (Units)	900 to 1,000	300 to 350	383 units allocated to Phase 3 of Water Tower Commons; 231 units allocated to K Mart redevelopment
Professional Office (SF)	20,000 to 30,000		Identify downtown infill parcels to accommodate at least 50% of future demand
General Industrial (SF)	40,000 to 50,000		Existing industrial is clustered in Lantana Industrial Park (Gator/Hillbrath Drives). Sites/locations to accommodate future industrial demand are unknown
Retail & Food Services (SF)			
- General Retail	7,000 to 12,000		Incremental demand generated by new residents. Other new retail will replace marginal performers or existing vacant space
- Food & Beverage	12,000 to 14,000		Unmet opportunities for new restaurants, but should be site-specific. Reinforce downtown 'dining district'.
- Consumer Services	Included in General Retail above		Over-supplied today. Future opportunities will replace marginal performers
Hotel/Lodging (Rooms) (Projects dependent on appropriate, available sites)	20 to 50		Boutique/destinational facility outside conventional market forces. Privately-financed; downtown site/location challenging. Water views desirable
	60 to 80		Limited-service national flag on major commercial corridor

Source: RDS; WTL+a, July 2022.

IMPACTS OF THE 2020—21 PANDEMIC

This report presents findings of demographic and real estate market conditions and development potentials in Lantana for housing, “workplace” (professional office, general industrial), and supporting uses such as retail and lodging. It should be noted that market conditions are based on data and conditions prior to and during the pandemic through year-end 2021. While the timing of future development may be more extended due to the pandemic, there are potentials for selected, well considered new growth and investment. Experience in other Florida markets has demonstrated that the best way to fully optimize economic benefits generated by redevelopment of selected, catalyst sites will result from a carefully structured and implemented plan that appropriately integrates different land uses and phases to provide development flexibility.

The complete analysis and discussion of the impacts of the COVID-19 Pandemic on the different market sectors, can be found in Appendix C of this report.

*Note: Since the completion of the Lantana Charrette Phase 3 of Water Tower Commons and the Kmart redevelopment proposal have been withdrawn. It is anticipated new or revised proposals for these properties will be submitted in the future.*



## KEY RECOMMENDATIONS

This master plan document attempts to address many of the community concerns raised through the public planning process. Design concepts, analysis, and suggested recommendations provided in Chapter II and III of this document include recommendations to:

- Invigorate investment in downtown Lantana along Ocean Avenue
- Provide well-designed additional parking that would support future reinvestment in the downtown
- Facilitate future redevelopment of underutilized parcels along US-1
- Provide bicycle and pedestrian improvements along US-1
- Manage the nature and scale of the redevelopment of larger parcels within the Town
- Plan medium and long-term improvements at Lantana Beach
- Explore options for retaining the historic architectural character of Lantana

Any master plan worthy of implementing is a living document; understanding that physical and market conditions are always changing as well as the community's appetite for change. While the proposals and concepts in this plan are detailed and specific, what is most important are the design principles embodied in the concepts and narrative of this report which are intended to help guide the Town through years of positive growth.



### SPECIAL PROJECTS

Through a significant public outreach process, including pre-charrette interviews, a Charrette Host Committee, and the public charrette itself, a number of issues and challenges to be addressed were brought to the attention of the team. The Saturday, July 9th public workshop, which kicked-off the charrette, was particularly useful as the community helped to identify the “Special Projects” that should be the focus of the analysis, design concepts, and key recommendations of this effort.

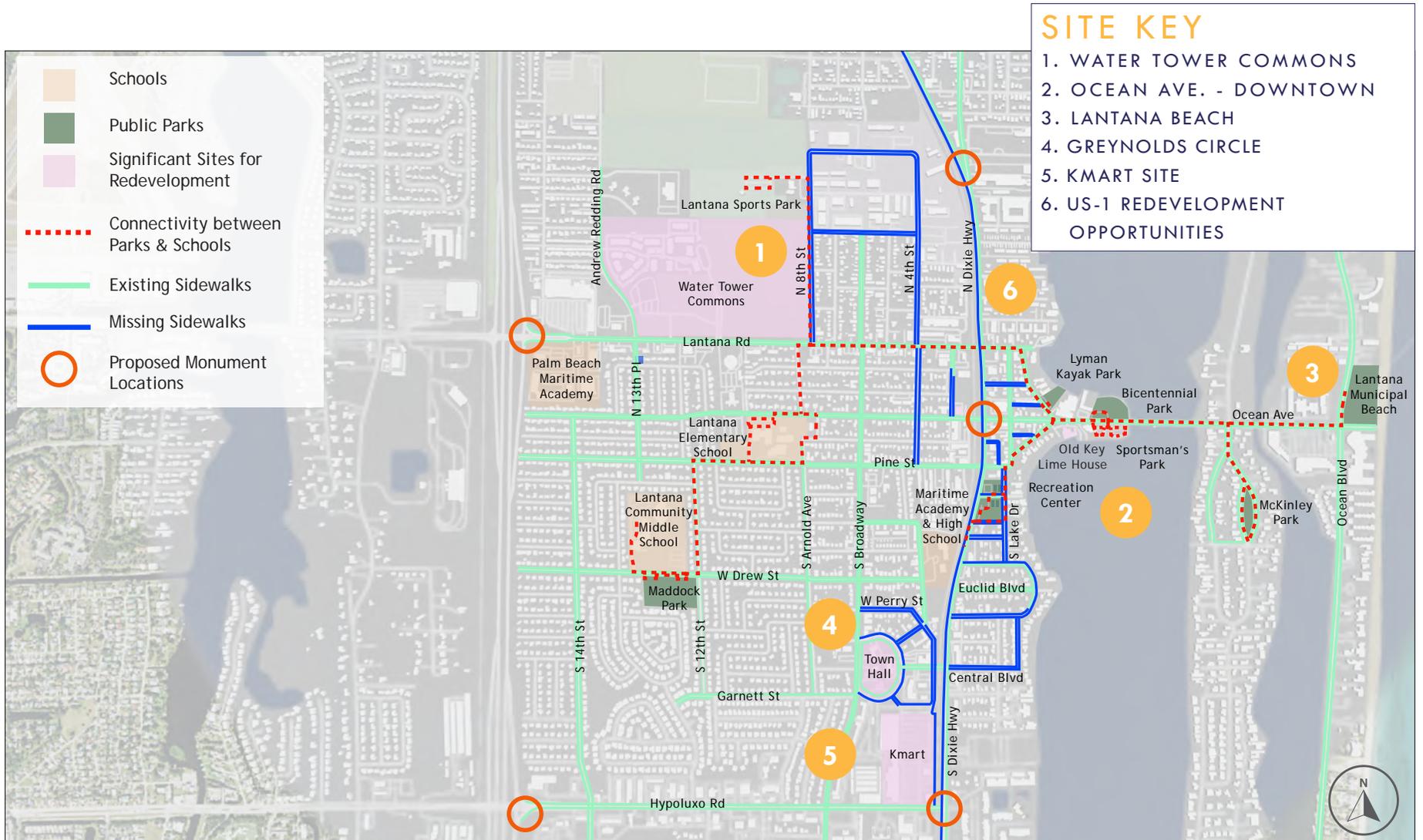
Once identified and prioritized, these special projects became the basis of the Master Plan drawing. Team members developed different levels of analysis, design and renderings, and specific recommendations for these key areas.

This chapter of the report provides a Tour of the Plan which illustrates the work developed for the special project areas. A Master Plan drawing, with a Site Key to project locations, is provided on the following page and further discussions and illustrations for each area are provided throughout the chapter.

This Master Plan report and its recommendations are intended to be a “living” document over time in that there will always be new challenges and a continuous stream of new special projects. The following is believed to be a solid foundation for redevelopment, infrastructure, and policy improvements for the Town which also provides a framework of design and planning principles to be implemented going forward.



## II. TOUR OF THE PLAN



### OVERVIEW OF THE PLAN

These five special project sites were developed in detail through the planning process. Each of the areas present unique and particular challenges however, the design principles of traditional planning have been applied to each condition. A detailed discussion of each of these special project areas is provided in the following pages.

SITE 1 - WATER TOWER COMMONS

1



SITE 2 - OCEAN AVE. - DOWNTOWN

2



SITE 3 - LANTANA BEACH

3



SITE 4 - GREYNOLDS CIRCLE

4



SITE 5 - K MART SITE

5



SITE 6 - US-1 REDEVELOPMENT

6

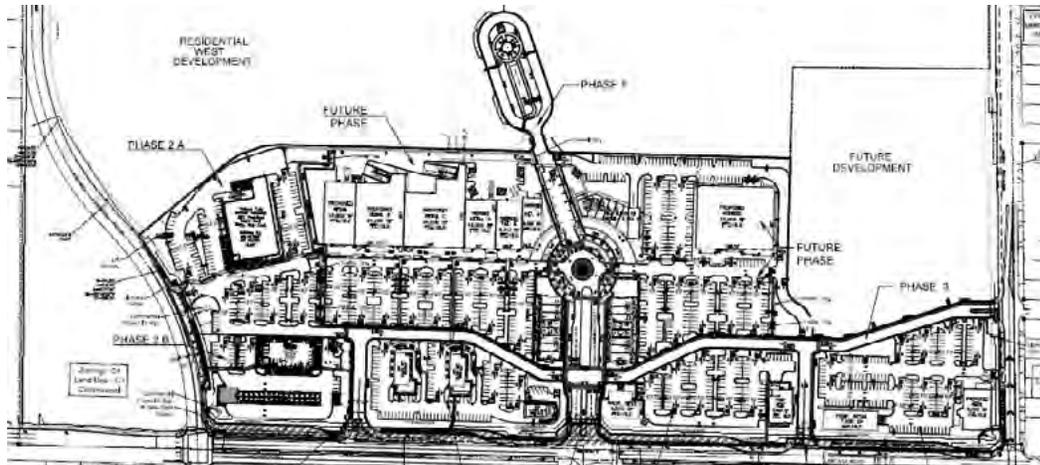


### WATER TOWER COMMONS

#### PROJECT HISTORY

Approved in 2014, Water Tower Commons is a mixed-use project that occupies the 73 acres that was formally the A. G. Holley State Hospital. Originally built in 1950 as the Southeast Florida Tuberculosis Hospital, Florida lawmakers voted to close the facility in 2012 which then sat vacant until the state agreed to sell the property to Boca Raton-based Southeast Legacy Investments for \$15 million.

The Phase I (Oceanside Lantana) 360 unit apartment complex broke ground in November 2018 and was occupied at the time of the charrette. Phase II (Manor Lantana) is complete and opened in 2021 with 348 units, for a total of 708 completed units. In part due to the Covid-19 pandemic and an uncertain retail market, the bulk of the commercial components have yet to be realized. Aldi supermarket, Wawa, and a Chick-fil-A are the current commercial elements.



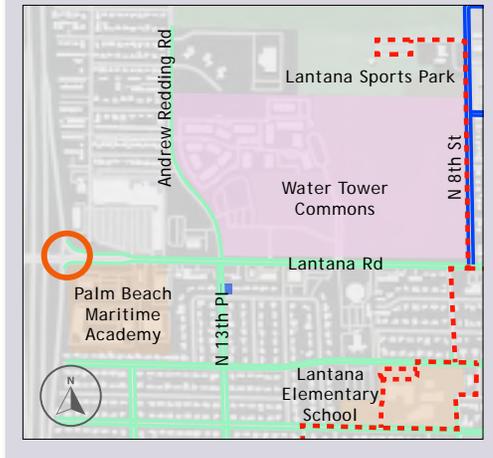
Existing approved site plan

#### WATER TOWER COMMONS

1

The Water Tower Commons is a 73 acre mixed use project with an approved program for:

- 1,091 residential units (708 units completed to date)
- 315,000 s.f. Commercial (Aldi and Wawa completed - Chick-fil-A under construction)
- Residential Phases I and II complete

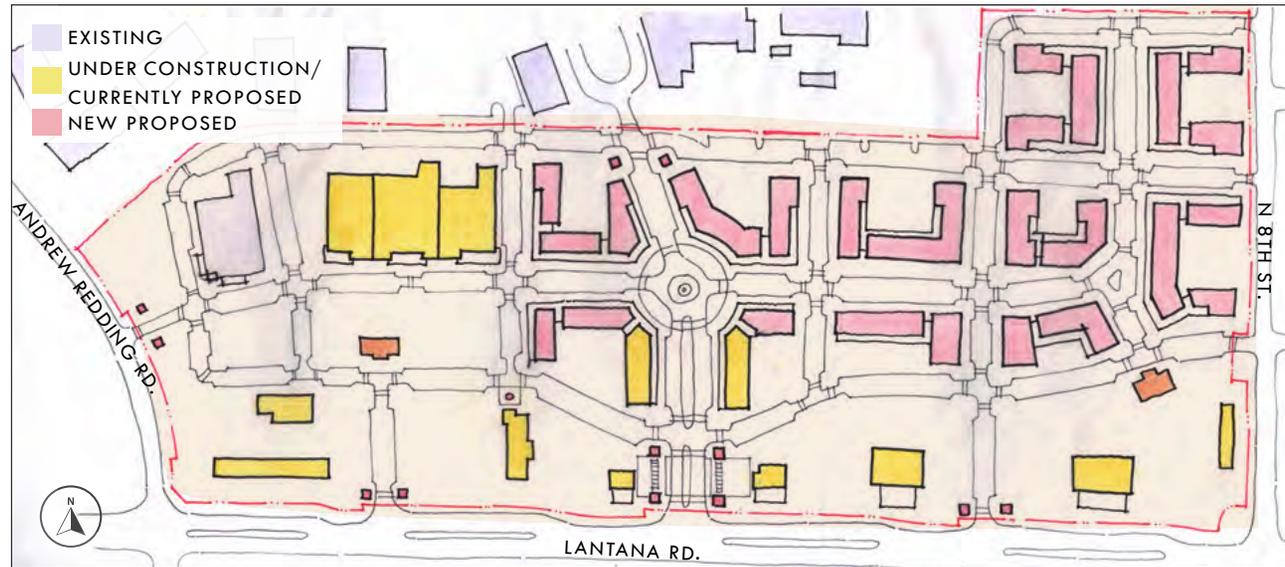


### WATER TOWER COMMONS

#### ALTERNATIVE PLAN

While the demand for residential remains strong and the implementation of the Water Tower Commons residential components seem to be on track, there has been a lag in the construction of the non-residential elements of the approved plan.

The full impacts of the pandemic on brick-and-mortar retail remain to be seen. In addition, the Town is “over-retailed” as identified in the Market Study conducted for this planning process. A concept for future development is illustrated below.



Water Tower Commons Concept

#### WATER TOWER COMMONS

1

In the event that the full retail/commercial component of the Water Tower Commons project cannot be implemented as planned due to a softening market, the design team recommends that the remaining phases of the project be reconfigured away from a conventional strip center model towards creating a main street with flexibility in the uses.



### WATER TOWER COMMONS



Water Tower Commons Concept

#### WATER TOWER COMMONS

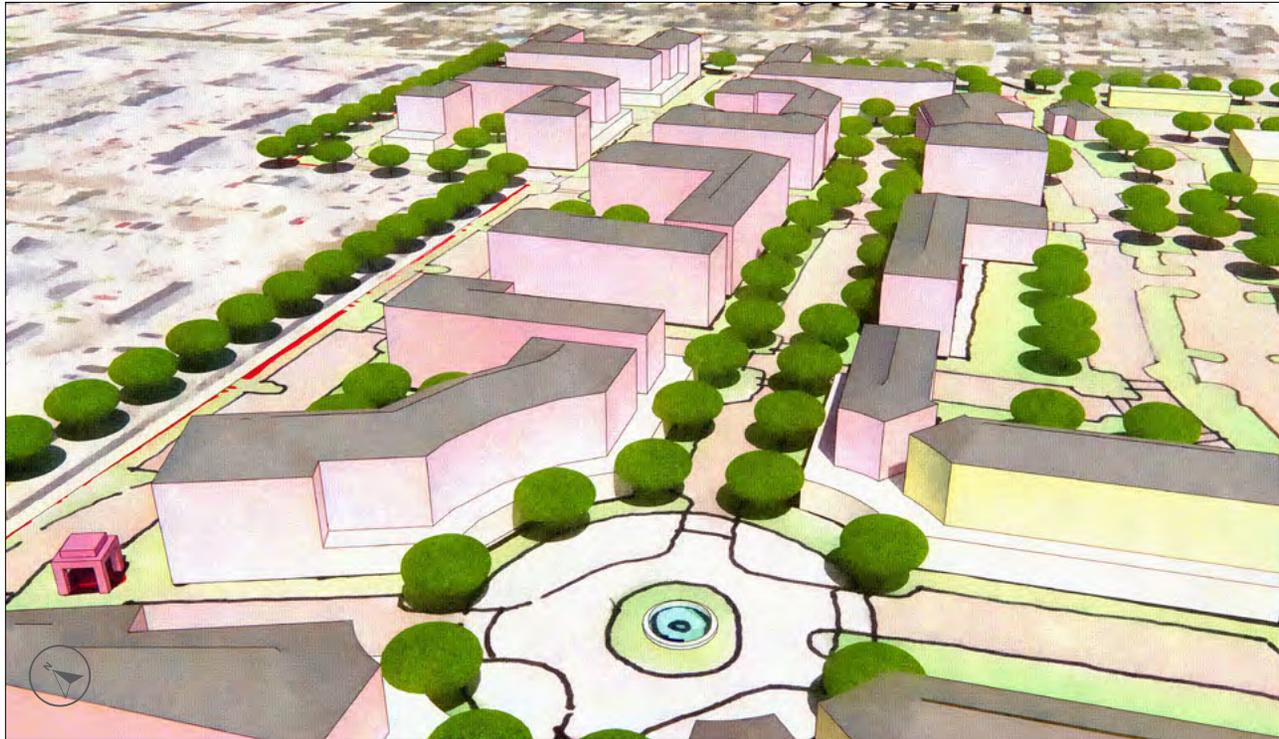
1

The rendering to the left illustrates a new east-west street springing from the existing roundabout framed by residential and mixed-use buildings.

The suggestion is that rather than trying to accommodate the larger format retail boxes as originally planned, for which there may no longer be a supportable market, create a place with more residential and mixed-use buildings.



### WATER TOWER COMMONS



Water Tower Commons Concept

#### WATER TOWER COMMONS

1

The image to the left shows the creation of a new, urban street with wide sidewalks, shade trees, and flexible uses in two-to-three story buildings.

This approach would create a desirable destination and provide flexibility in the marketplace. This approach would require modifications to the approved master plan and should be considered by the Town Council if requested.



### OCEAN AVE. - DOWNTOWN POTENTIAL IMPROVEMENTS

A key focus of the community during the charrette process was Lantana’s downtown on Ocean Avenue between N Dixie Highway and the foot of the Lantana bridge. While only 1,500’ in length, which includes Bicentennial Park and Sportsman’s Park and boat ramps, the Lantana main street is consistent with the typical length of historic main streets around the country.

The challenges to downtown as raised during the planning process include parking, the provision of more street trees, and the reluctance of some property owners to improve their properties.

Additional recommendations regarding downtown can be found in Chapter III “Downtown” on page 78.



### OCEAN AVE. DOWNTOWN

2

Downtown Lantana is essentially the main street of Ocean Avenue from N Dixie Highway east to the bridge.

Lantana’s main street configuration is similar to that of Boynton Beach: the historic main street is the primary access to the beach but is three blocks south of the primary I-95 access (Lantana Road in this case).



### OCEAN AVE. - DOWNTOWN



Examples of existing commercial structures along Ocean Avenue

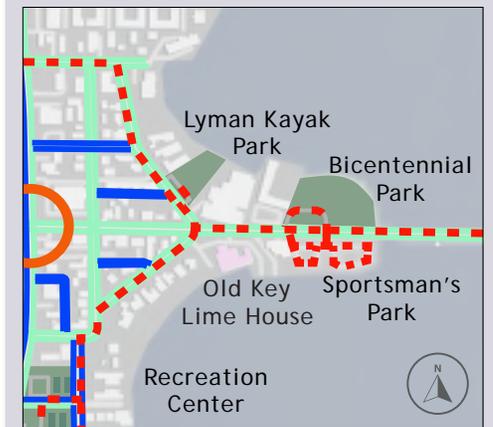
An important issue that was discussed during the charrette is the flooding potential for some of these structures which are clearly lower than the crown of road. As challenges with sea-level rise become more persistent, the raising or replacement of these structures needs to be considered. If they are to be replaced, the character they currently provide to the street must be retained.

### OCEAN AVE. DOWNTOWN

2

A number of smaller commercial structures of differing styles line Ocean Avenue and provide a charming and quaint character to the street. Unfortunately a number of these have remained vacant for some time and are in need of repair.

One challenge of this planning effort is to develop strategies to motivate property owners to make improvements.



### OCEAN AVE. - DOWNTOWN

#### EXISTING CONDITIONS



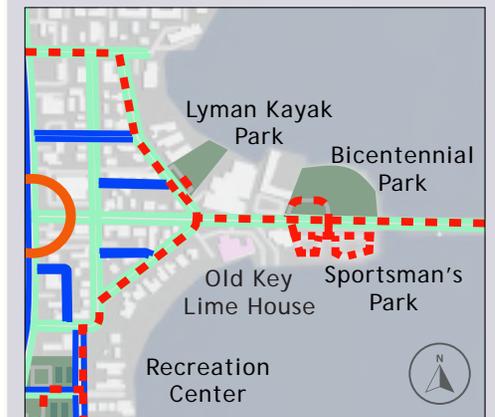
- |                             |                                    |
|-----------------------------|------------------------------------|
| <b>A</b> Old Key Lime House | <b>C</b> Lantana Bicentennial Park |
| <b>B</b> Lyman Kayak Park   | <b>D</b> Lantana Boat Ramp         |

### OCEAN AVE. DOWNTOWN

2

The drawing to the left illustrates the existing conditions of downtown Lantana today. The key at the bottom of the drawing highlights some of the main destinations in downtown.

A primary challenge in downtown is how to promote revitalization and incremental infill and provide the necessary parking while maintaining the quaint character of the place.



### OCEAN AVE. - DOWNTOWN INFILL OPPORTUNITIES



**OPTION 1**

### OCEAN AVE. DOWNTOWN

2

**OPTION 1**

- 5 Units
- 3 story
- Garage Apartments (5)

Recommendations include increasing building heights from 2 stories to 3 stories as part of an incentive program. Currently the cottage commercial sub-area maximum building height is limited to 25'.



### OCEAN AVE. - DOWNTOWN INFILL OPPORTUNITIES



#### OPTION 2

#### OCEAN AVE. DOWNTOWN

2

##### OPTION 2

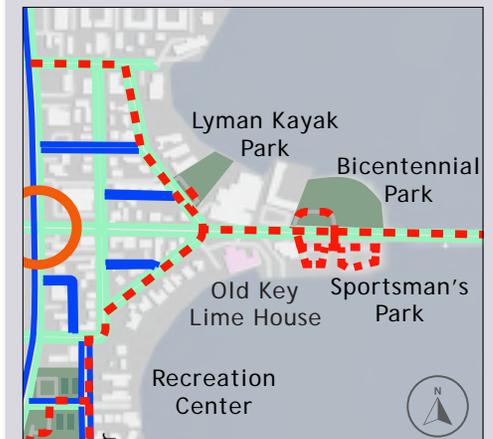
2 Historic Structures preserved  
(yellow footprints)

On-Street Parking

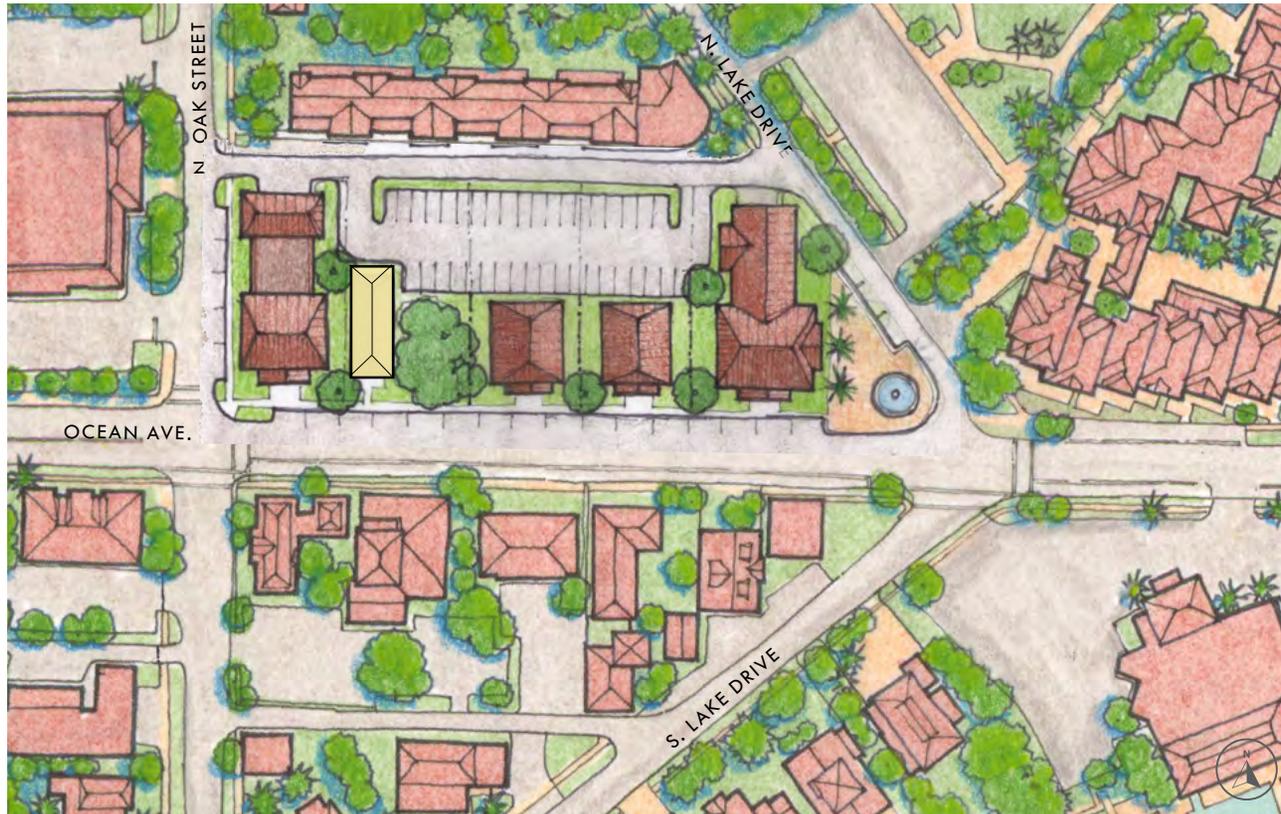
3 Story infill buildings

Ground Floor Retail/Commercial

Residential 2nd & 3rd Stories



### OCEAN AVE. - DOWNTOWN INFILL OPPORTUNITIES



#### OPTION 3

#### OCEAN AVE. DOWNTOWN

2

##### OPTION 3

##### Mixed-Use 3 Story Infill Buildings

1 Historic Structure Preserved (yellow)

Parking provided in the rear

Each of these redevelopment options include the provision of a new, small public plaza at the corner of N Lake Drive and Ocean Avenue.



### OCEAN AVE. - DOWNTOWN INFILL OPPORTUNITIES



Artist rendering of Ocean Ave. view north near the intersection with North Lake Drive.

### OCEAN AVE. DOWNTOWN 2

The rendering to the left depicts two new three-story infill buildings facing Ocean Avenue immediately east of an existing cottage proposed (in this scenario) to be preserved.

A balance between preserving existing structures with the greatest architectural and historical character and building new will, in part, depend upon market and financial forces.



### OCEAN AVE. - DOWNTOWN INFILL OPPORTUNITIES



Artist rendering of the intersection of North Lake Drive and Ocean Ave. This view is looking south with the existing Old Key Lime House in the background.

### OCEAN AVE. DOWNTOWN

2

There are a few obvious destinations that already exist in downtown Lantana (the Old Key Lime House being one of the premier destinations) with new ones in the works.

One of the greatest challenges to improving downtown will be maintaining the scale and character that is so unique to Ocean Avenue yet also invigorate investment in the area. This plan will provide some recommendations on how to achieve that balance through strategic modification to the downtown codes.

The image to the left is of the proposed N Lake Drive and Ocean Avenue plaza.

OCEAN AVE. - DOWNTOWN  
DOWNTOWN OPPORTUNITIES

**2**

OCEAN AVE. DOWNTOWN

The Town of Lantana Downtown Overlay Zone is a set of zoning regulations crafted specifically for the downtown area. These rules are broken into six different sub-areas intended to reflect the unique characteristics of each area. The map to the left is the official Downtown Overlay Zone map which identifies the sub-area locations.

Generally, the provisions provided within the Overlay Zone are satisfactory in their intent. However, updates could clarify and strengthen the requirements.

More importantly, strategies for incentivizing investment through limited duration programs should be considered as part of the overlay zone.

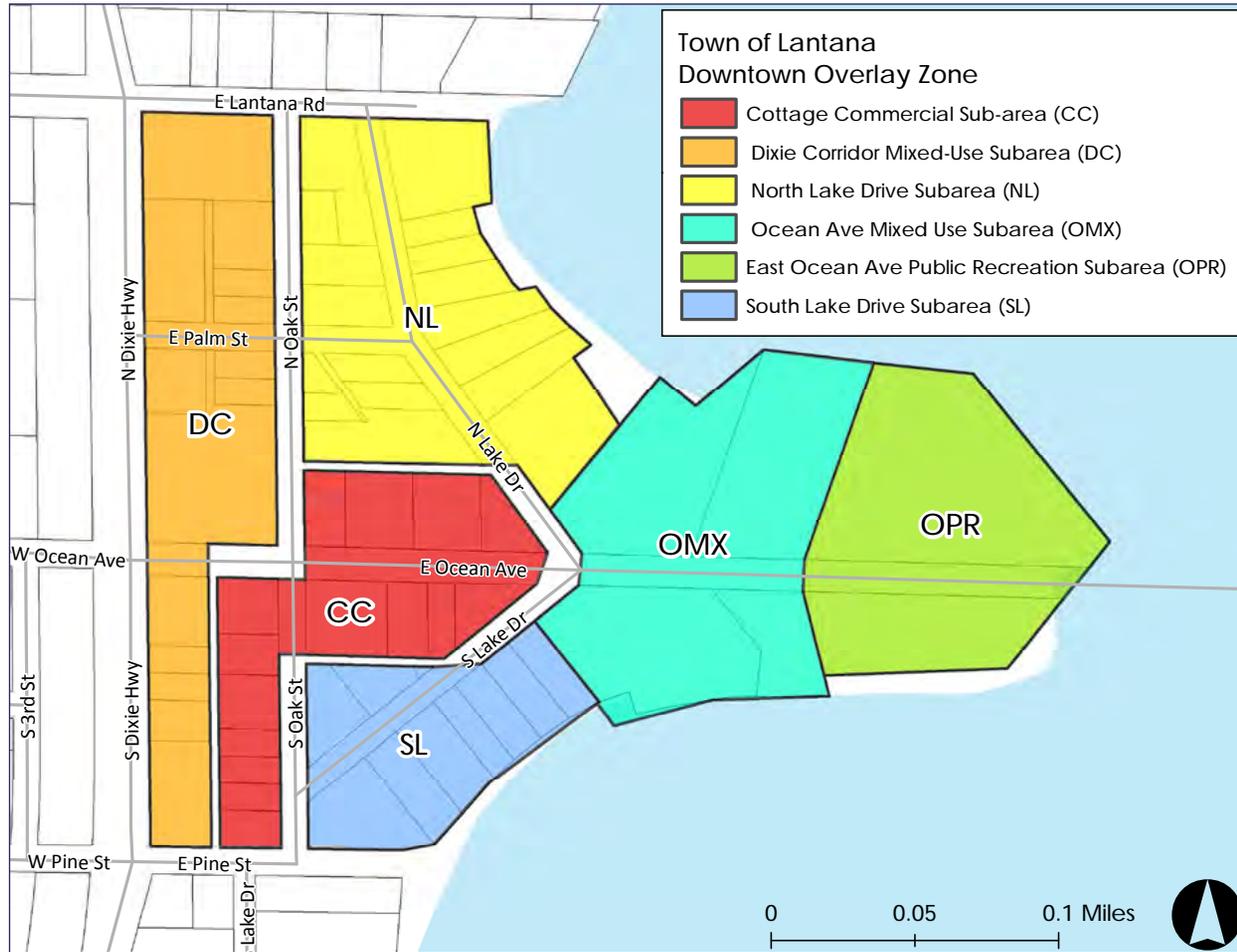


FIGURE 01 TOWN OF LANTANA DOWNTOWN OVERLAY ZONING MAP



### OCEAN AVE. - DOWNTOWN PARKING

Parking is a “problem”, perceived or real, in virtually every town; especially those that have a successful commercial center or main street. Downtown Lantana and Ocean Avenue are no exception. With a regional destination like the Old Key Lime House on Ocean Avenue there will be pulses of parking demand and occasionally limited availability. The key is providing enough parking to maintain business viability without blowing up the character of the place which made it desirable to begin with. Unfortunately many towns and cities have erred on the side of providing voluminous parking without a full consideration of the impact parking lots and garages have on the scale and character of the place.

This plan will consider various ways of addressing parking issues in downtown.



Image looking East along Ocean Ave.

### OCEAN AVE. DOWNTOWN

2

Ocean Avenue in downtown Lantana benefits greatly from the approximately 55 on-street parallel parking spaces that line the Avenue. These spaces are essential to the vitality of downtown businesses: typically on-street parking spaces generate significant revenue in sales to the businesses they front.

Interestingly, the bulk of the Ocean Avenue on-street parking is largely due to the alley system that exists mid-block. By providing access to the rear of the properties and removing the need for driveways and curb-cuts along Ocean Avenue, the corridor can provide the greatest amount of on-street parking. Maintaining an alley system and on-street parking in downtown is vital going forward.



### OCEAN AVE. - DOWNTOWN PARKING



**FIGURE 02** TOWN OF LANTANA DOWNTOWN PARKING ANALYSIS

### OCEAN AVE. DOWNTOWN

2

An analysis was conducted of the existing parking areas which are shown in red while the existing buildings are shown in black on the figure-ground image to the left. On-street parking is in addition to these areas.

Creating this diagram helps the team understand the quantity and location of existing off-street parking relative to demands.



### OCEAN AVE. - DOWNTOWN ON-STREET PARKING



### OCEAN AVE. DOWNTOWN

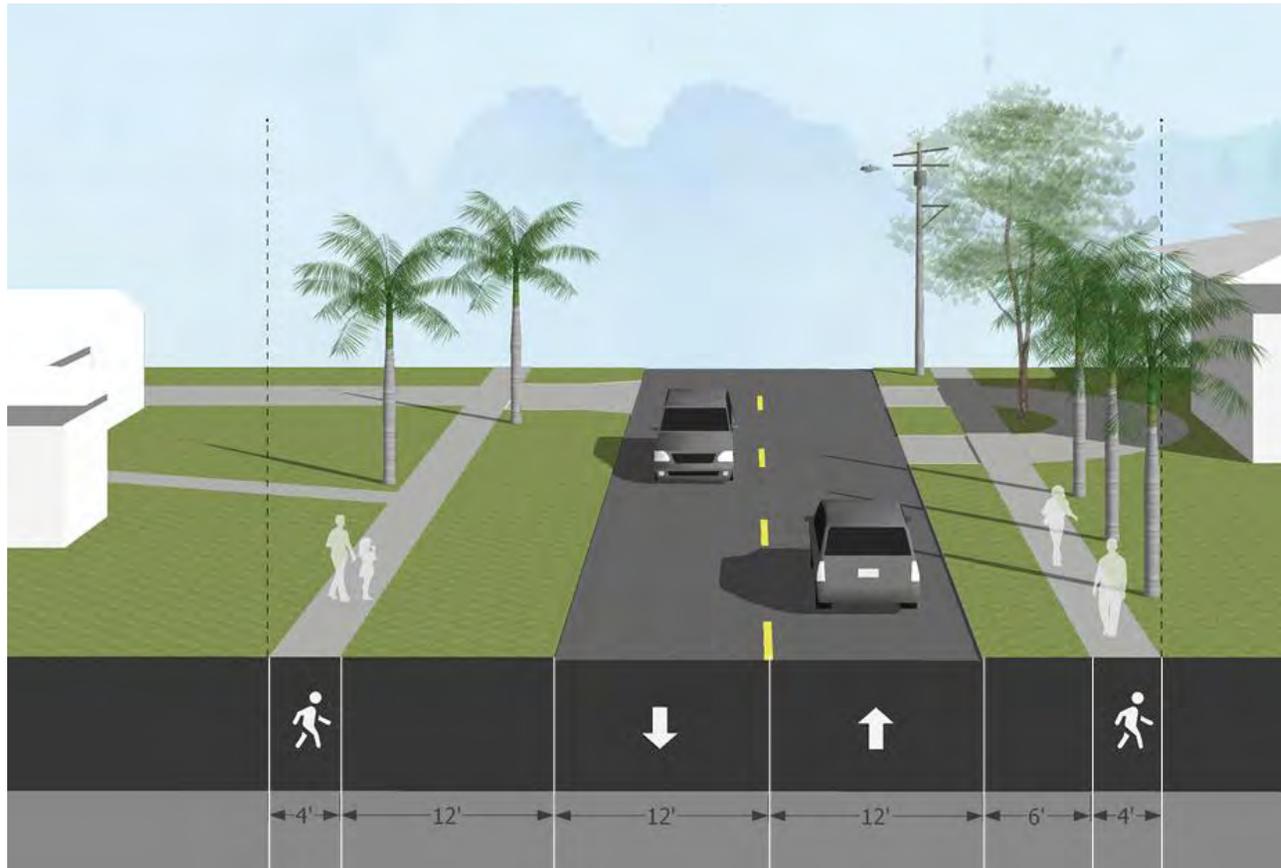
2

One approach to creating additional downtown parking spaces is to explore the creation of new on-street spaces. The following pages illustrate existing and proposed conditions on S. Lake Drive and S. Oak Street which, if reconfigured, could yield the following:

33 additional spaces along S. Lake Drive and 13 along S. Oak Street.



### OCEAN AVE. - DOWNTOWN PARKING



**FIGURE 03** EXISTING CONDITIONS OF SOUTH LAKE DRIVE

### OCEAN AVE. DOWNTOWN

2

The diagram to the left illustrates the existing conditions on S. Lake Drive. Note the deep swales between the edge of street and the sidewalks. The Town could incrementally add additional on-street parallel parking between the existing sidewalks and pavement, with interruptions for driveways and landscaping.



### OCEAN AVE. - DOWNTOWN PARKING



**FIGURE 04** OPTION A: RECOMMENDED IMPROVEMENTS FOR SOUTH LAKE DRIVE

### OCEAN AVE. DOWNTOWN

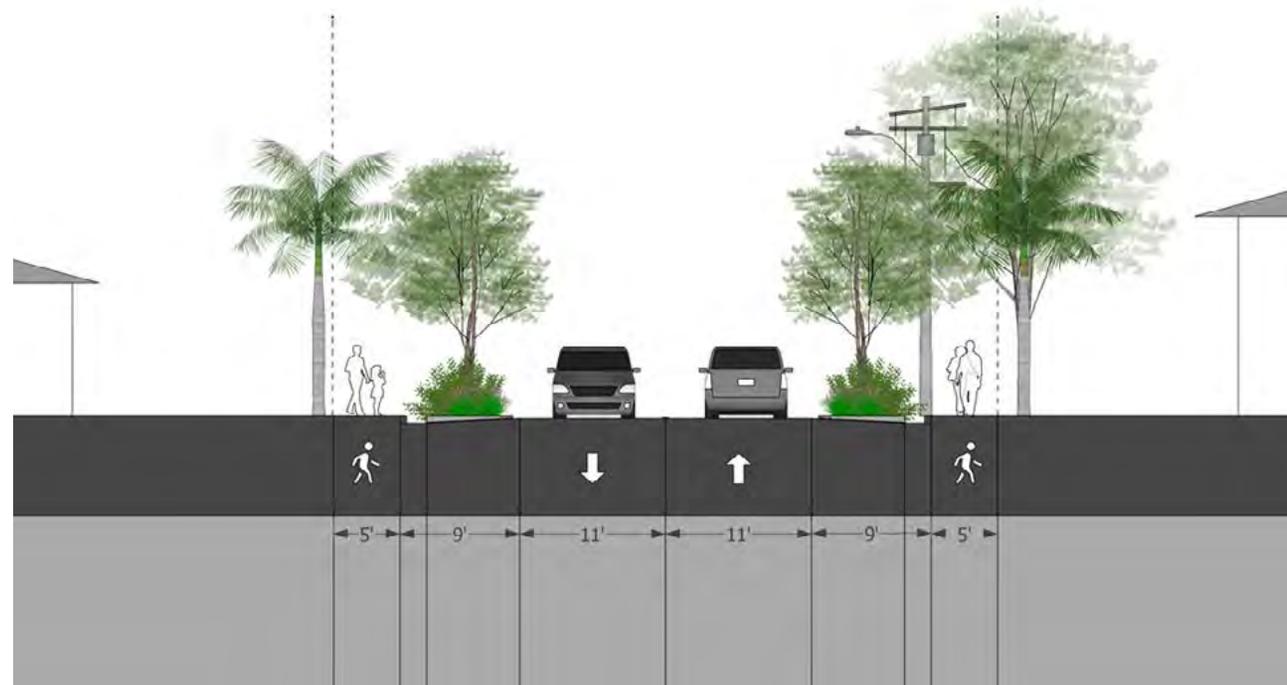
2

The proposed conditions diagram at left shows how additional parking can be provided between the exiting sidewalks and edge of roadway.

As there are already some drainage issues within the area, the preferred treatment is for a structured pervious system to support vehicles yet allow for drainage into the soil. Landscape islands will soften the visual impact of the parking and allow for rain gardens for additional drainage.



### OCEAN AVE. - DOWNTOWN PARKING



**FIGURE 05** OPTION A: RECOMMENDED IMPROVEMENTS FOR SOUTH LAKE DRIVE

### OCEAN AVE. DOWNTOWN

2

The cross section to the left provides the dimensional criteria to the proposed on-street parking solution for S. Lake Drive.

While this approach could provide over 30 parking spaces which would definitely benefit downtown businesses, this proposal should be fully discussed and vetted with the community, especially those who live on S. Lake Drive.



OCEAN AVE. - DOWNTOWN  
PARKING

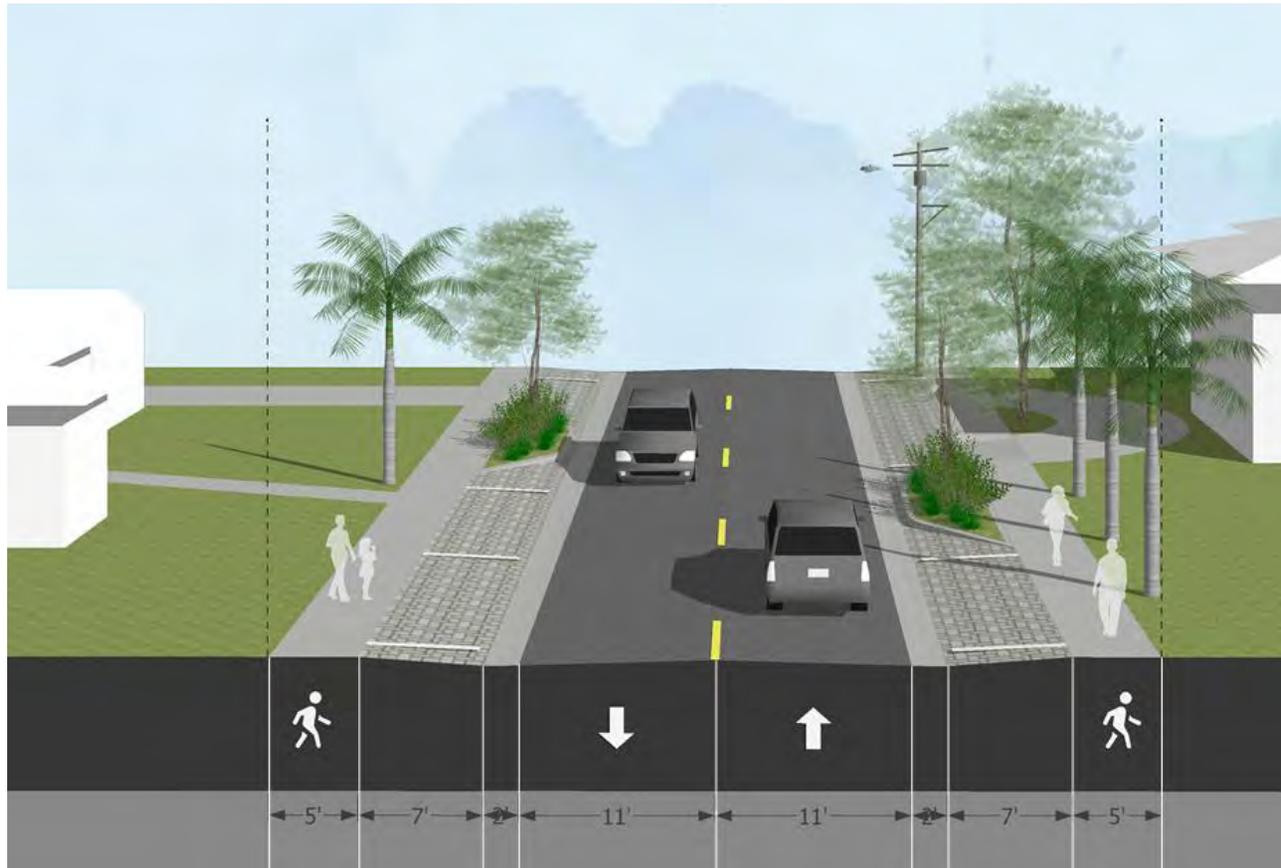


FIGURE 06 OPTION B: RECOMMENDED IMPROVEMENTS FOR SOUTH LAKE DRIVE

OCEAN AVE.  
DOWNTOWN

2

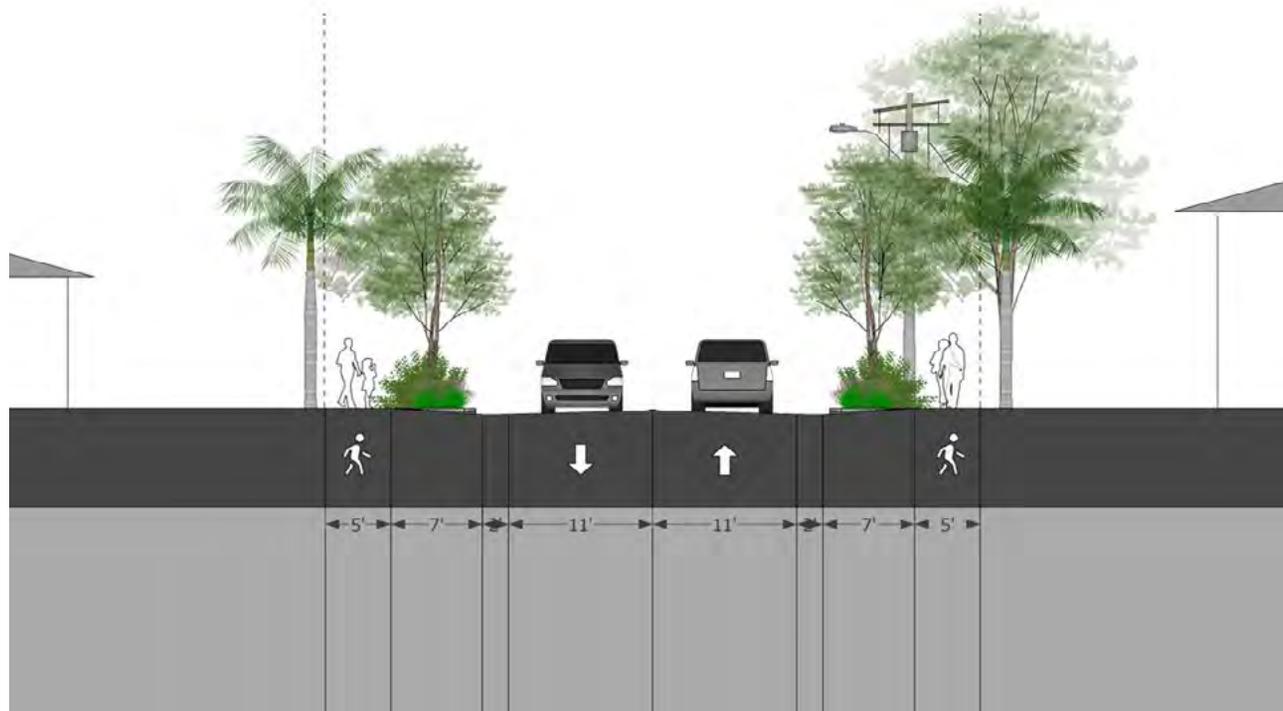
At left is a diagram of an alternate version B for the S. Lake Drive on-street parking proposal.

Option A on the previous pages includes a raised curb and gutter at the sidewalk level requiring more grading.

Option B at left keeps the existing sidewalk as is and proposes the on-street spaces grade towards the street and into valley gutters.



### OCEAN AVE. - DOWNTOWN PARKING



**FIGURE 07** OPTION B: RECOMMENDED IMPROVEMENTS FOR SOUTH LAKE DRIVE

### OCEAN AVE. DOWNTOWN

2

Option B would probably be a less intrusive project for the neighborhood as there would not be the need to raise the existing sidewalks. The benefits to Option A with the raised sidewalks is a bit more protection for pedestrians and a greater degree of discipline by those parking on-street (typically motorists will not park on the sidewalk if it is raised).

### OCEAN AVE. - DOWNTOWN ADDITIONAL PARKING



**FIGURE 08** ADDITIONAL PARKING LOCATION MAP

Along with the 3rd North and 3rd South parking areas identified above, the 1.1 acre site could serve as surface or structured parking in the future.

### OCEAN AVE. DOWNTOWN

2

One idea that came up during the charrette is the potential for creating a concentration of publicly-accessible parking spaces just outside of downtown proper.

Just north of Ocean Avenue and west of the FEC tracks is a large vacant parcel that among other redevelopment uses, could potentially provide additional parking. The site is about a five-minute walk from downtown.

Recommendations and considerations for the existing Town owned parking areas can be found in Chapter III “Downtown dining incentive area” on page 79.

### LANTANA BEACH

#### INCREMENTAL IMPROVEMENTS

There was much discussion during the charrette process regarding the current condition of the beach park and amenities. The beach has a popular regional dining destination in the Dune Deck Cafe. One of the very few places in Palm Beach County where one can eat in a restaurant environment at the beach's edge, Dune Deck is an institution in Lantana.

How to build upon the Dune Deck activity and provide greater amenities for beach-goers was the focus of the beach improvement discussion.

Additional improvements recommended can be found in Chapter III "Incremental Improvements" on page 100.



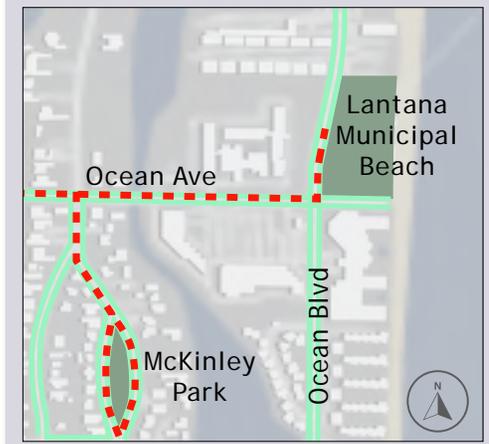
### LANTANA BEACH

3

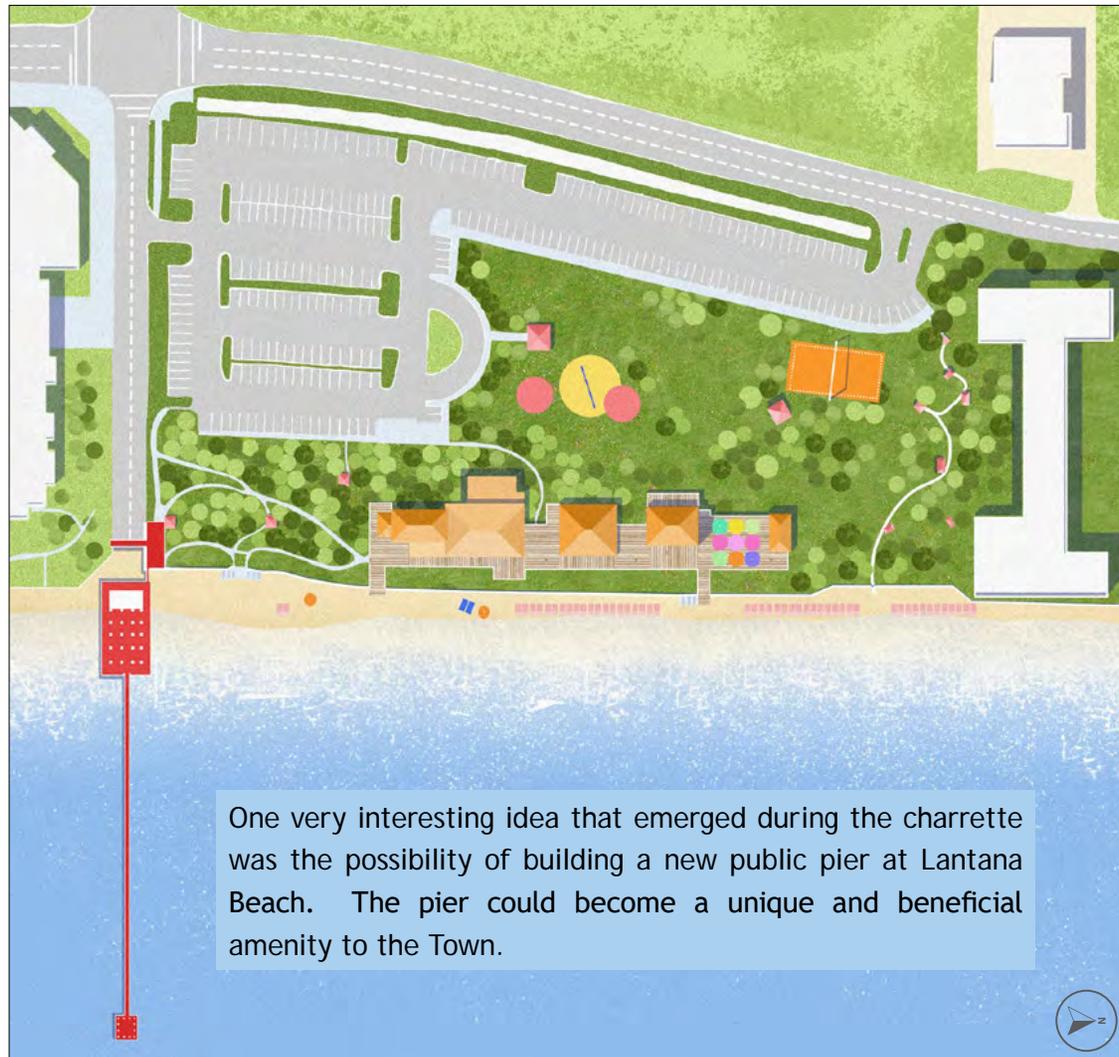
One interesting aspect to the Lantana municipal beach front is the seawall and the significant change in elevation between the dune area and the beach strand.

One resident said, "we don't really have a beach a lot of the time - we have an ocean."

There was much discussion about how to activate the water side of the dune as well.



### LANTANA BEACH



### LANTANA BEACH

3

The drawing to the left illustrates a proposed concept for beach improvements. The plan proposes to extend the deck at Dune Deck to the north, and replace the existing vacated pavilion with new shade and sundry/convenience store structures.

The current edge of the Dune Deck deck would not be extended any further eastward.



### LANTANA BEACH

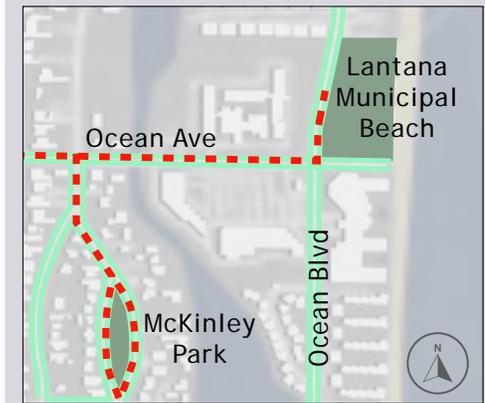


### LANTANA BEACH

3

The image to the left shows three new structures north of the Dune Deck restaurant along an expansive area of new decking.

One structure is two stories with the potential for a banquet/gathering space on the second level and sundry shops on the ground level, and open air are pavilion immediately north for additional picnic or refreshment/ice cream seating, and new restrooms to the north.



### LANTANA BEACH

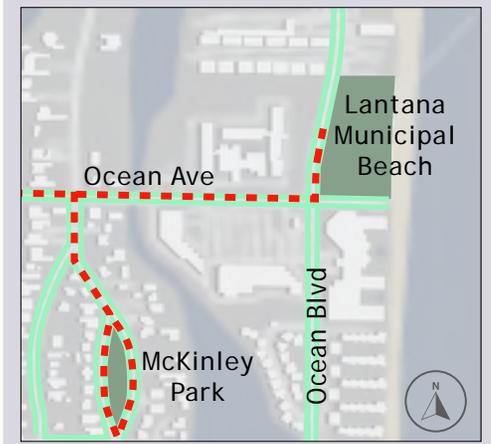


#### LANTANA BEACH

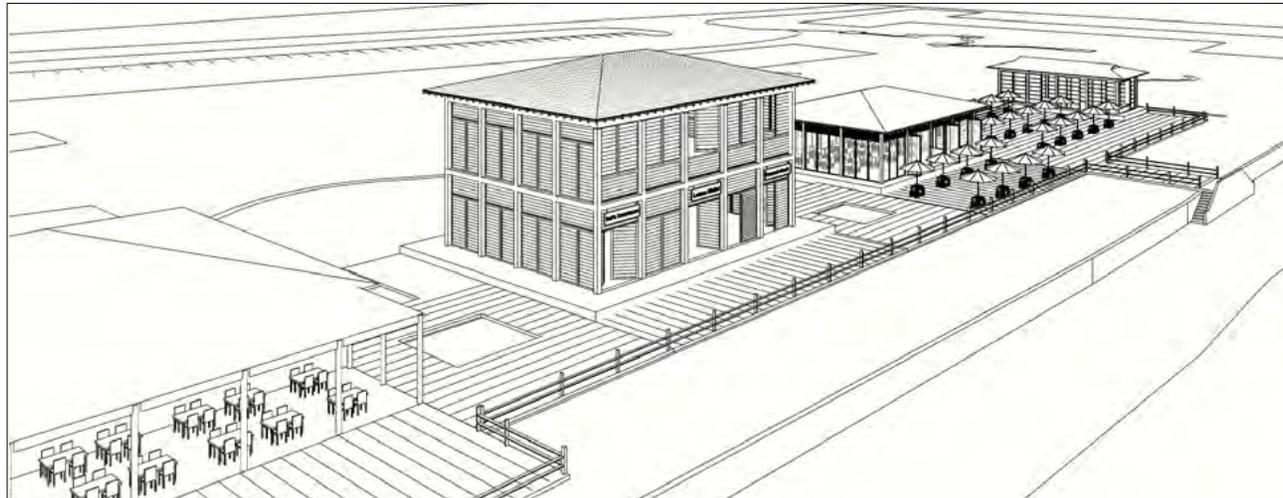
3

This is a deck-level view looking south towards the existing Dune Deck restaurant.

The two-story structure beyond the open air pavilion has the observation/banquet space on the second story with a beach side convenience store below that could sell beach sundries, ice cream, water, etc. The proposed pier can be seen in the distance at the south end of Lantana Beach



### LANTANA BEACH

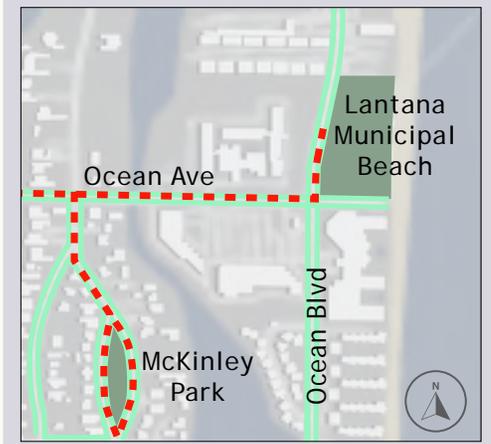


### LANTANA BEACH

3

To the left are additional views of the proposed deck and facility expansion at Lantana beach.

While there was public support for limited commercial and structure expansion, there is a great sensitivity to over-building or over-commercializing this cherished public asset.



### GREYNOLDS CIRCLE

#### A NEW CIVIC CAMPUS

The Town of Lantana Town Hall and administrative functions occupy three 1960's era buildings in a campus-like arrangement ringed by Greynolds Circle between Dixie Highway and S. Broadway. In addition to the Town Hall, Development Services, and Administration buildings, Palm Beach County Fire Rescue Services (Fire Station #37) is also part of the campus of buildings.

While the existing buildings are quite dated, the campus grounds themselves are beautiful and have great potential for future enhancements.

Connectivity improvements for Greynolds Circle can be found in Chapter III "Greynolds Circle Connectivity Improvements" on page 92.

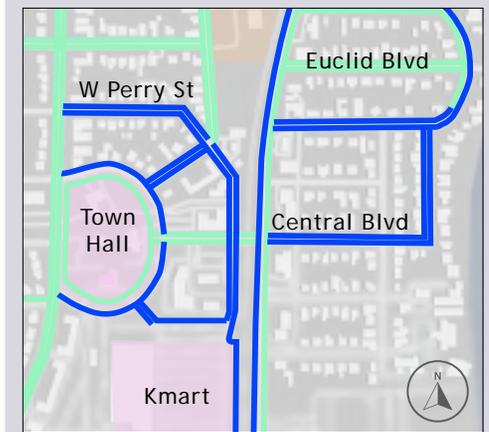


Image of Lantana Town Hall

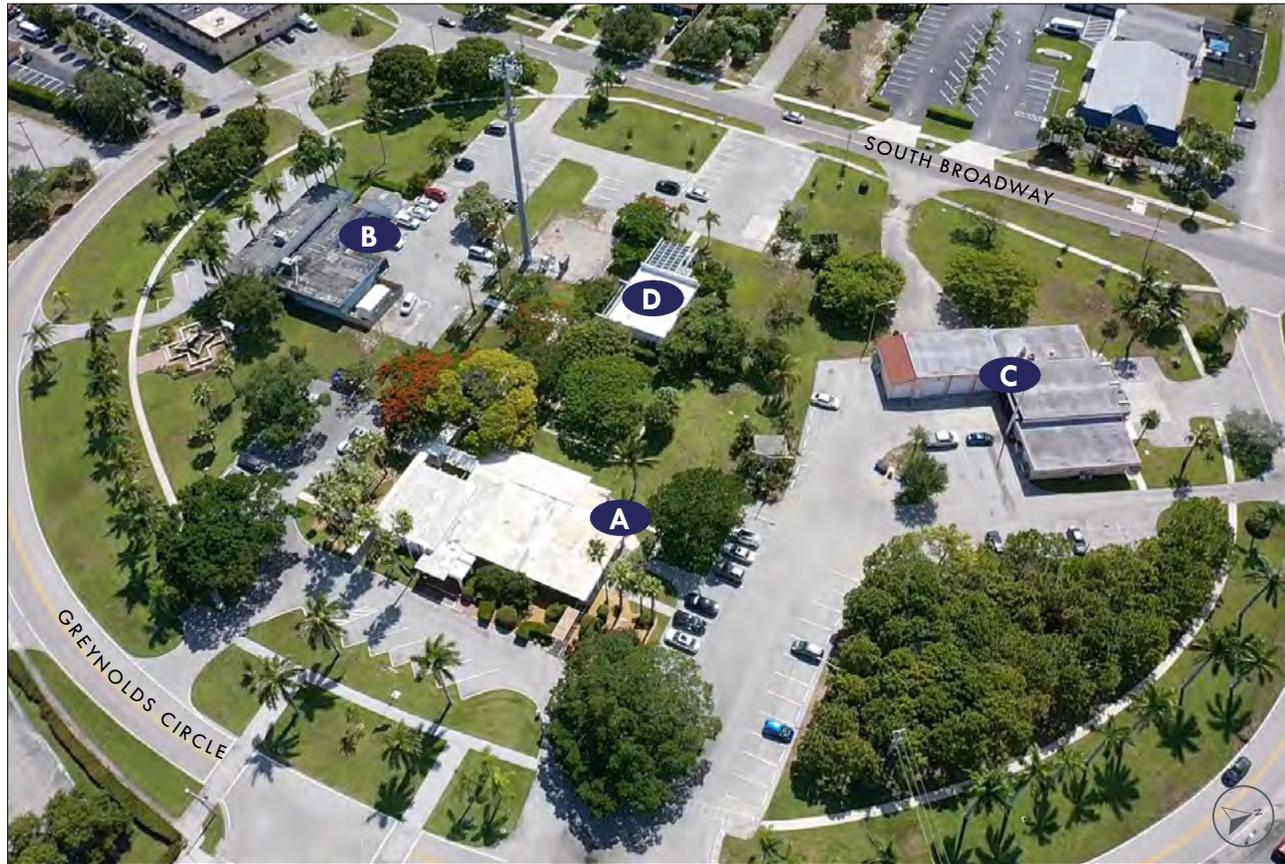
#### A NEW CIVIC CAMPUS

4

The Town of Lantana municipal campus is quite unique for Palm Beach County. Clearly originally designed to be a civic site, the campus has a central location and is approached from South Dixie Highway on Central Boulevard which has the Town Hall as its terminus. This formal relationship is rarely found and should be celebrated and retained over time even if a new Town Hall is built.



### GREYNOLDS CIRCLE



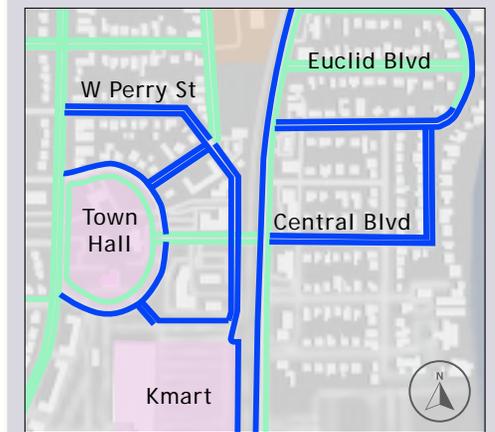
- A** Town Hall
- B** Development Services
- C** Fire Rescue Station
- D** Administration

### A NEW CIVIC CAMPUS

4

The aerial image to the left gives a good overview of the Lantana municipal campus and the assemblage of buildings.

Currently there is little interaction or spatial relationship between the buildings and in fact traveling between them as a pedestrian can be a bit of a challenge. The disjointed buildings also impact administrative efficiency.



### GREYNOLDS CIRCLE

#### TOWN HALL



#### OPTION 1

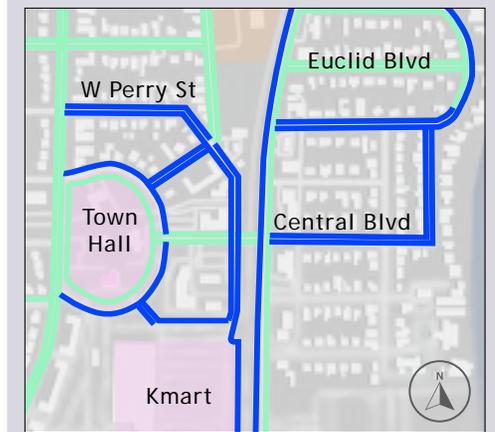
*The concept above could be built while maintaining use of the existing Town Hall.*

#### A NEW CIVIC CAMPUS

4

In an effort to memorialize the discussions had during the charrette pertaining to the Lantana municipal campus, the team developed a couple of long-term proposals for a new Town Hall facility and campus revitalization.

The version to the left rebuilds Town Hall in more or less its current location.



# GREYNOLDS CIRCLE TOWN HALL



## OPTION 1



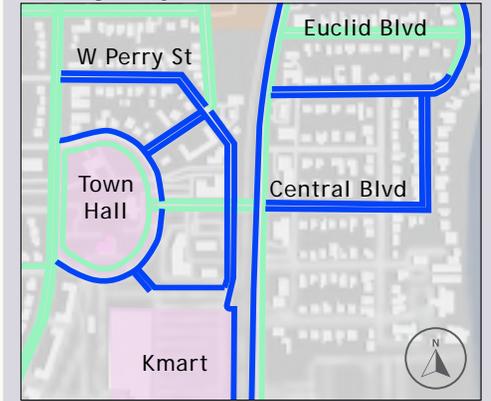
## A NEW CIVIC CAMPUS

4

The rendering to the left shows a new Town Hall maintaining the existing formal approach and axial relationship to Central Boulevard which is enhanced with new buildings on adjacent properties framing the street.

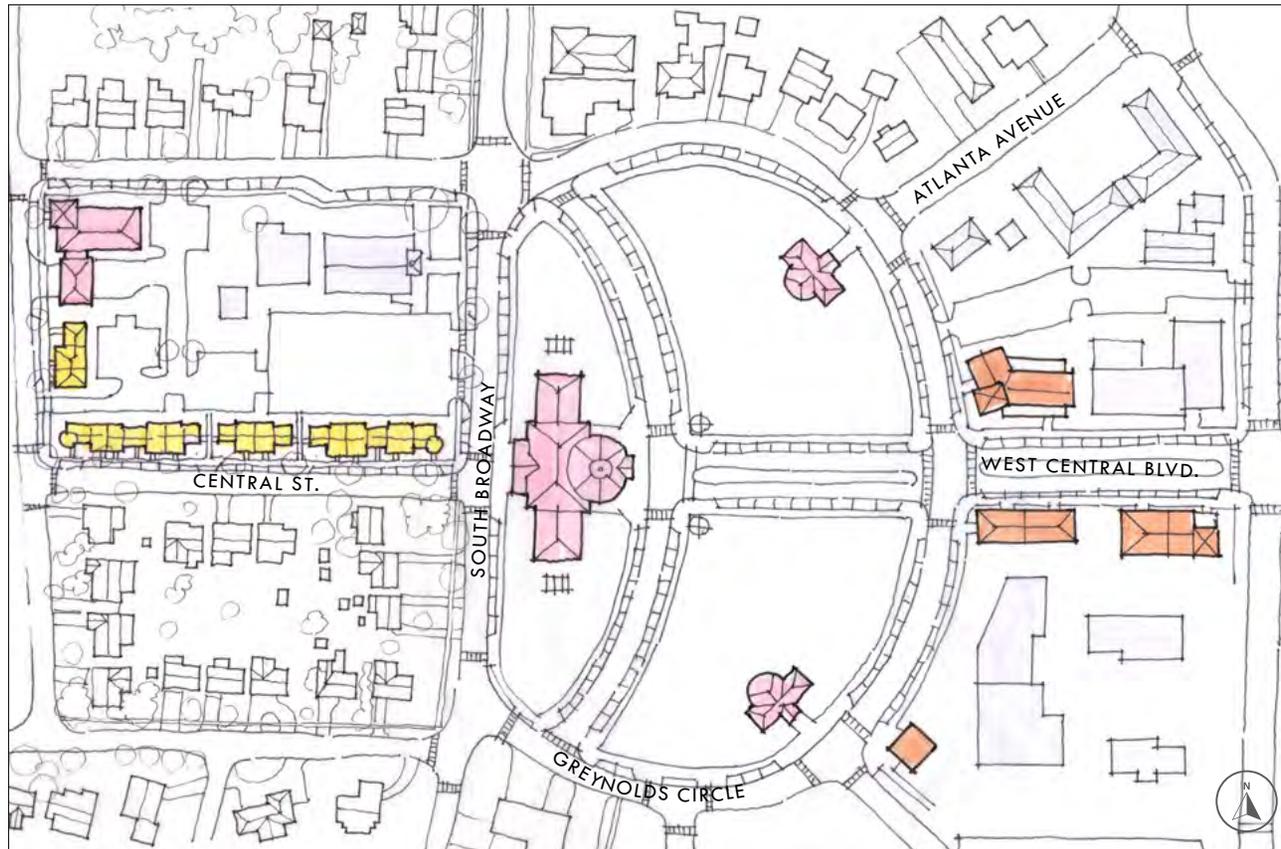
The Fire Station remains and the balance of the campus is restored to passive park space which could be utilized for events.

There is on-street parking all along Greynolds Circle.



### GREYNOLDS CIRCLE

#### TOWN HALL



#### OPTION 2

### A NEW CIVIC CAMPUS 4

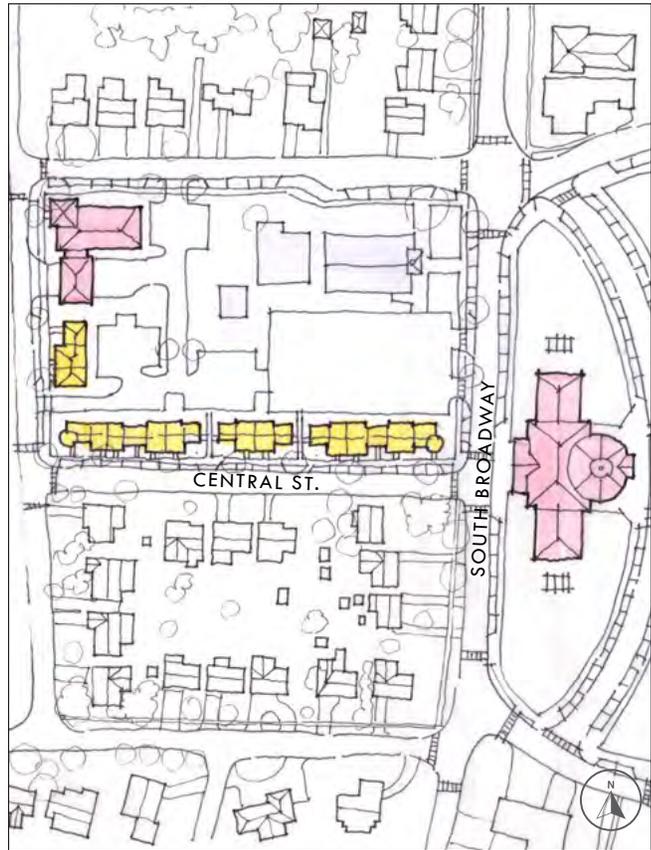
Another Town Hall campus concept was developed. This scheme, at left, maintains the Town Hall as the terminus to Central Boulevard but pushes the new structure further west towards S Broadway. This would enable a new Town Hall be built while still occupying the existing Town Hall and Development Services during construction.

The concept also contemplates building a replacement fire station one block west, which would involve acquiring property.



### GREYNOLDS CIRCLE

#### TOWN HALL



#### OPTION 2



Images of townhouses with a narrow lot condition in West Palm Beach

#### A NEW CIVIC CAMPUS

4

The multi-story townhouse building type is very versatile and can be effectively used to screen the public realm from parking lots, garages, and less desirable urban conditions. The townhouses proposed on Central Street are intended to screen the large church parking lot to the north and provide continuity along the street. These sites are approximately 50' deep. As an example of this solution, the townhouse photos to the left screen a historic neighborhood in West Palm Beach from the back of the Palm Beach County Convention Center. These townhouses exist on lot depths of less than 50'.

### GREYNOLDS CIRCLE

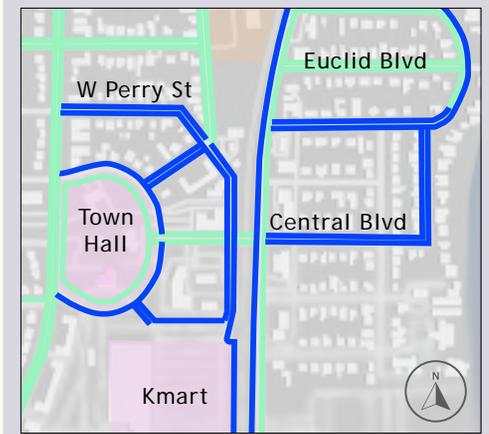
#### TOWN HALL



#### OPTION 2

### A NEW CIVIC CAMPUS 4

The rendered plan to the left shows how Central Boulevard would extend through the site creating two new formal lawns to the north and south. The structures shown within the new lawns would be formal open-air pavilions that could house a stage and certain components of festivals and events. The new roadways as well as Greynolds Circle have on-street parking.



### GREYNOLDS CIRCLE

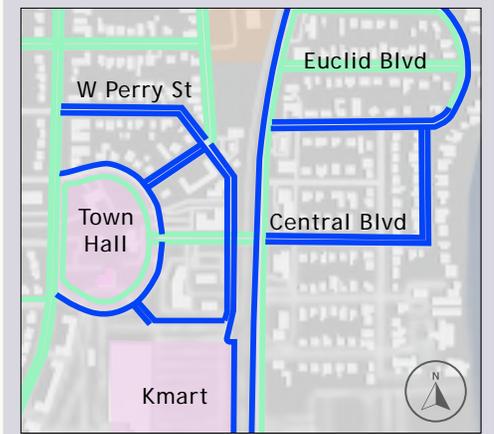
#### TOWN HALL



#### OPTION 2

#### A NEW CIVIC CAMPUS 4

This aerial view of the alternate design proposal emphasizes the strong axis of the Central Boulevard extension which terminates in the new Town Hall. Both design concepts developed for the future of Greynolds Circle and the Town Hall campus strive to build upon the elements of the existing campus that are unique to the Town of Lantana.



### KMART/WINN DIXIE SITE

#### EXISTING CONDITIONS



Existing conditions of the Kmart Site. North is to the left.

**KMART SITE** 5

Outside of the Water Tower Commons mixed-use project, the single largest redevelopment opportunity in the Town of Lantana is the Kmart/Winn Dixie site at the NW corner of South Dixie Highway/US-1 and Hypoluxo Road.

This approximately 17.6 acre parcel includes the now vacant Kmart, a Winn Dixie grocery store, West Marine, and other in-line shops. It is immediately south-east of the Town Hall campus.



### KMART/WINN DIXIE SITE EXISTING CONDITIONS



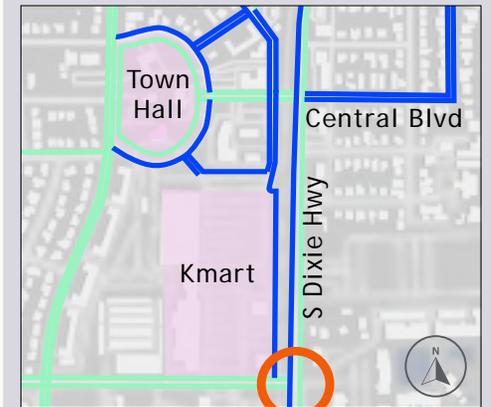
Views all looking south at the Kmart site.

### KMART SITE

5

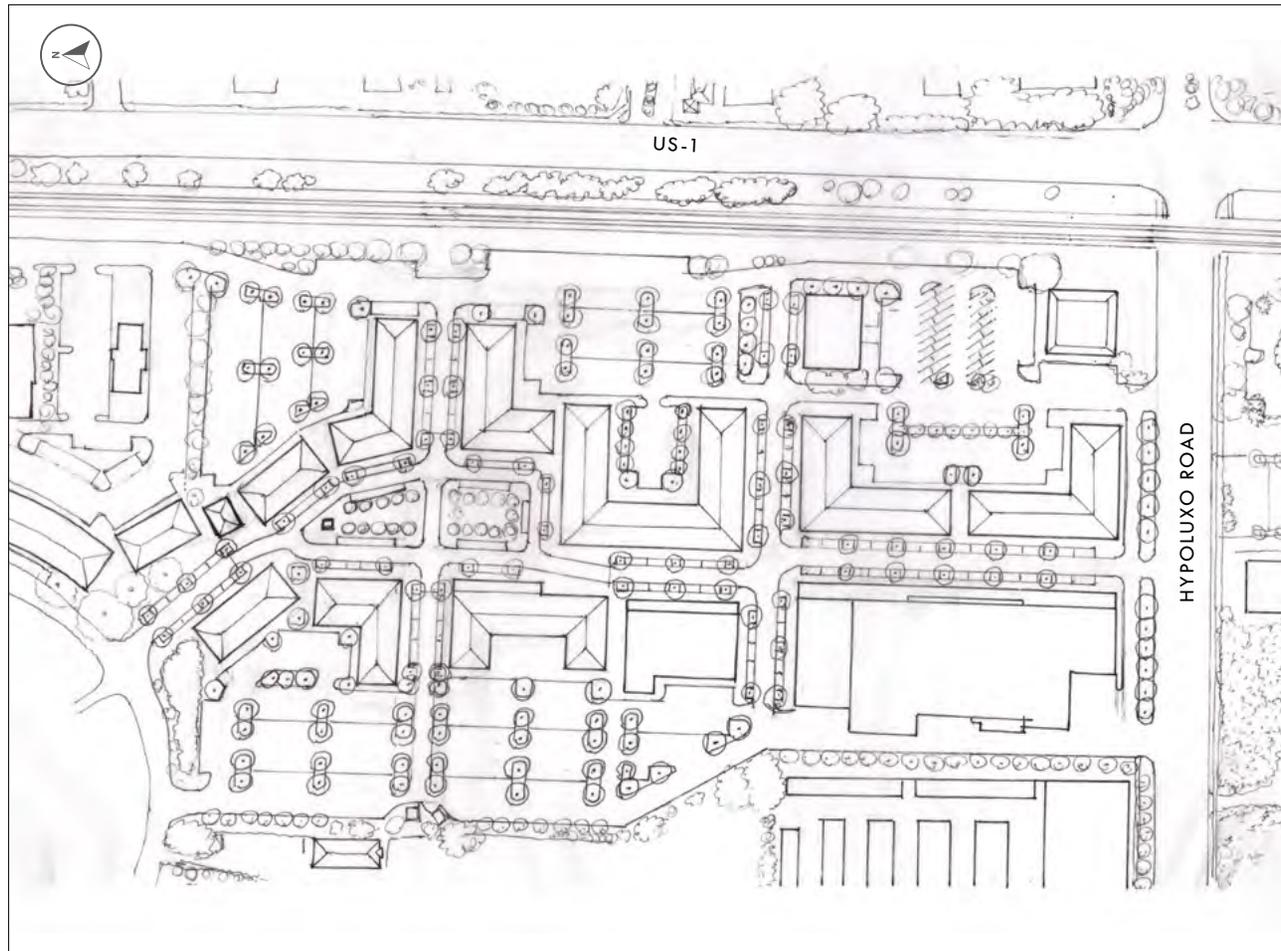
The images to the left show the Kmart building itself (northern most building with white roof) and the vast surface parking area at the plaza.

Winn Dixie, which is still in operation, is at the southern end of the plaza.



### KMART/WINN DIXIE SITE

#### DESIGN CONCEPT

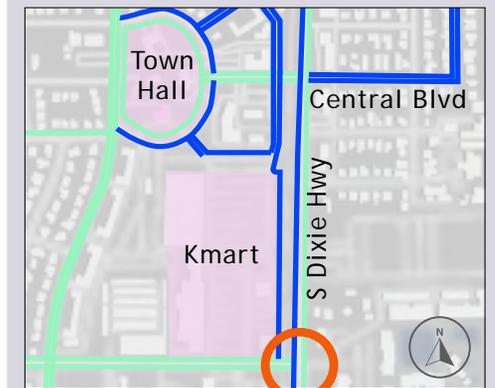


#### KMART SITE

5

The Town has considered a few different proposals for portions of the Kmart/Winn Dixie Site however there has not been a comprehensive approach for the entire site to redevelop over time.

Understanding the complexities of existing lease arrangements etc., the design team developed a concept that illustrates a phased redevelopment over time. A nearly fully redeveloped site is depicted in the sketch at left



# KMART/WINN DIXIE SITE

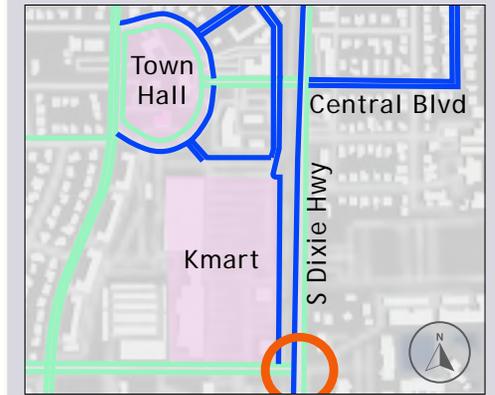
## DESIGN CONCEPT



### PHASE 1

## KMART SITE 5

The rendered plan view to the left illustrates a redeveloped Kmart site at the north end of the plaza which includes mixed-use and residential apartment/condo buildings facing a central green and park to enhance the sense of community. The parking is on-street and to the rear of the new buildings, and the beginnings of a formal street that connect Greynolds Circle and Hypoluxo Road is emerging.



### KMART/WINN DIXIE SITE

#### DESIGN CONCEPT



#### PHASE 2

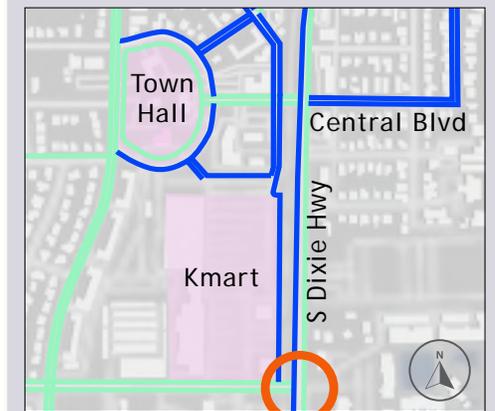


5

#### KMART SITE

This intermediate phase illustrates two new buildings built immediately east (in front of) the existing Winn Dixie. While this may not be a likely scenario if Winn Dixie is still in operation, the point is to show how redevelopment can occur incrementally.

These new structures reinforce the new N-S street connecting Greynolds Circle and Hypoluxo Road.



### KMART SITE CONCEPT



### PHASE 3

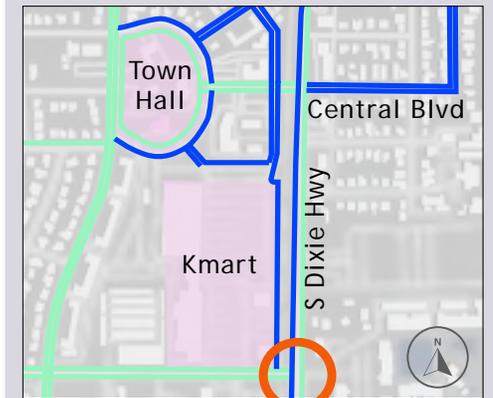


### KMART SITE

5

This drawing shows the fully implemented plan with the removal of Winn Dixie and the other in-line shops. The connection between Greynolds Circle and Hypoluxo Road is complete with a street lined with sidewalks, on-street parking, and shade trees.

The bulk of the parking is behind the buildings.



### KMART SITE CONCEPT



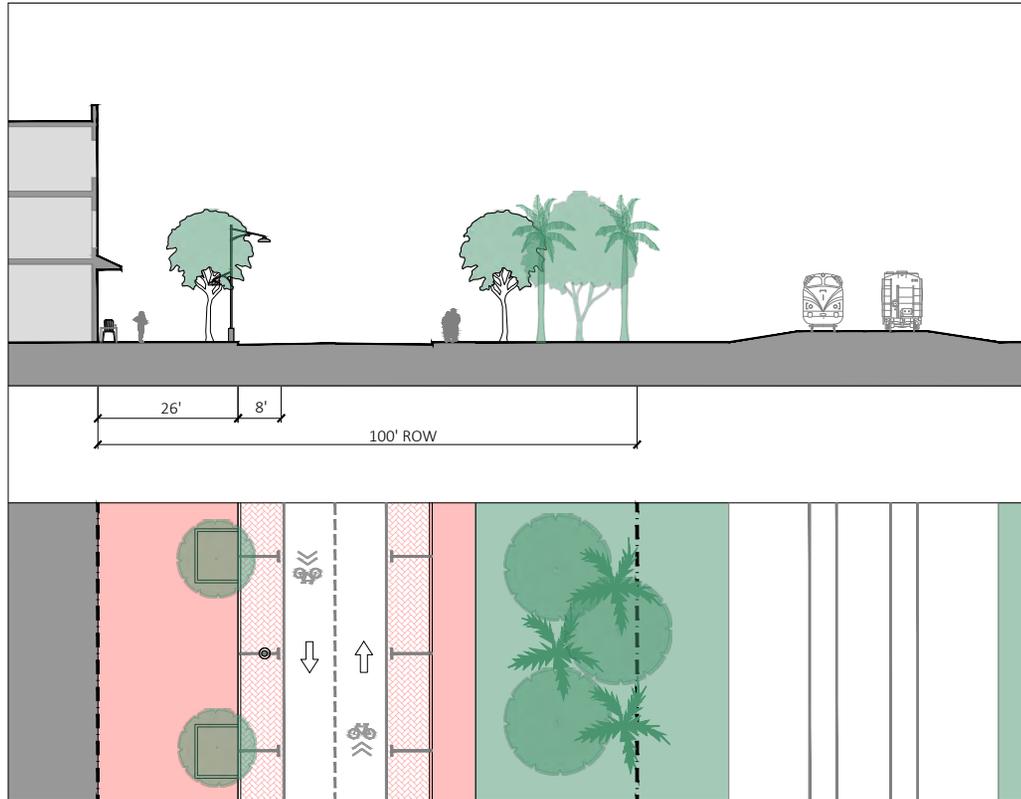
#### KMART SITE

5

An alternate design concept was developed for the site which takes advantage of the FEC rail corridor frontage to the east. As passenger rail service has been re-established on the FEC corridor with the introduction of Brightline, for the first time in decades the average citizen has a unique vantage point to observe cities from West Palm Beach to Miami. When one travels on Brightline it becomes apparent that there are few locations where the city fabric faces the corridor; most of the rail corridor travels along the backs of buildings and commercial properties. The Kmart site has the opportunity to provide an interesting and welcoming façade to the rail service. This alternate design proposal seeks to take advantage of this condition.

KMART SITE

FIGURE 09 FEC RAILWAY AT KMART SITE



FEC RAILWAY

The street and corridor cross-section illustrated to the left shows development to the left (west) facing a new roadway along the rail corridor with on-street parking and landscape parkways to the east (right). The railway tracks and train cars are illustrated to the far right (east). Note the wide sidewalks along the building frontage, two-way travel lanes, and landscape buffers. This condition is comparable to the Brightline service corridor in downtown West Palm Beach.



### KMART/WINN DIXIE SITE CONCEPT



Artist rendering of the redevelopment concept for the Kmart site.

#### KMART SITE

5

This view is from above Greynolds Circle at Town Hall looking south towards the new redevelopment.

Note that all of the buildings face the new N-S street and there are no gates or fences.

Integrating future redevelopment (and future residents) at this location into the existing fabric of the town is of paramount importance. During the charrette, the community expressed opposition to isolating this site.



View of existing conditions

# KMART/WINN DIXIE SITE

## CONCEPT



Artist rendering of the redevelopment concept for the Kmart site.

**5**

### KMART SITE

The view at left is also looking south towards the new redevelopment. The rendered view is built upon the existing conditions photo below where you can see the existing Kmart building relative to the new buildings to the left.

The rendering gives a glimpse into the central square and green surrounded by mixed-use and residential multi-story buildings.



View of existing conditions



KMART/WINN DIXIE SITE

DESIGN CONCEPT

KMART SITE

5



This rendering illustrates the central green proposed for the Kmart and Winn Dixie parcels with a through street south to Hypoluxo Road.



### TOWN IMPROVEMENTS

#### ARCHITECTURAL CHARACTER

While primarily a bedroom community historically, the Town of Lantana has an interesting array of architectural styles, many of which could be considered historic.

Preserving the architectural heritage of the Town is a topic that was discussed a great deal during the charrette process. The Town may not be ready to consider bona-fide historic districts at this point, however identifying and recognizing the architectural traditions of the community will be beneficial in the long run.

The Town should continue to consider the possibility of creating historic districts for contributing structures within the Town as a way to preserve its history.



West Lantana Road

#### ARCHITECTURAL STYLES



Mid-Century Modern



Florida Bungalow



Wood Frame Vernacular

### TOWN IMPROVEMENTS ARCHITECTURAL CHARACTER



416 West Lantana Road

The improvements above include articulating the original windows as fixed and awning pane, providing sills consistent with the eyebrow, providing planter boxes across the frontage and introducing signage above.



812 West Lantana Road

The improvements above include removing the mirrored treatments from the windows, replacing the fixed, non-operable shutters with usable awnings, articulating the entry gable and columns more clearly, and providing for a new roof material that supports the “Small Fishing Village” theme that is supported by the Town.



### TOWN IMPROVEMENTS

Often even buildings that have historical character or represent a particular historical style, if not protected, can be modified and changed over time in ways that diminish their original character. In addition, buildings that do not have historical value but could be improved architecturally are present.

These and the following sketches illustrate before and after conditions of how existing buildings might be modified to improve their appearance and connection to the street.



### TOWN IMPROVEMENTS

#### ARCHITECTURAL CHARACTER



#### TOWN IMPROVEMENTS

The image below is the existing condition of a building that even though it has awnings it has no windows. The sketch to the left shows how the addition of two windows to complement the awnings and facade, in addition to other small details, can improve the building's image and street presence.

Small enhancements such as these would go a long way in improving aesthetics in the Town and signaling to the developments community.



445 West Lantana Road

### TOWN IMPROVEMENTS GATEWAYS AND MONUMENTS



Town of Lantana connectivity map with proposed monument locations highlighted in red.

### TOWN IMPROVEMENTS

During the charrette there was discussion about the provision of entry or gateway features at strategic locations within the Town. The map to the left identifies prioritized monument locations circled in red.

Not surprisingly these sites are located along Dixie Highway, Lantana Road, and Hypoluxo Road at major entry points to the town as well as on Ocean Avenue at the entry to “downtown”.



### TOWN IMPROVEMENTS GATEWAYS AND MONUMENTS



View looking East along Ocean Avenue near U.S.-1



Worth Avenue Clock Tower, Palm Beach



Prado Entry, Coral Gables

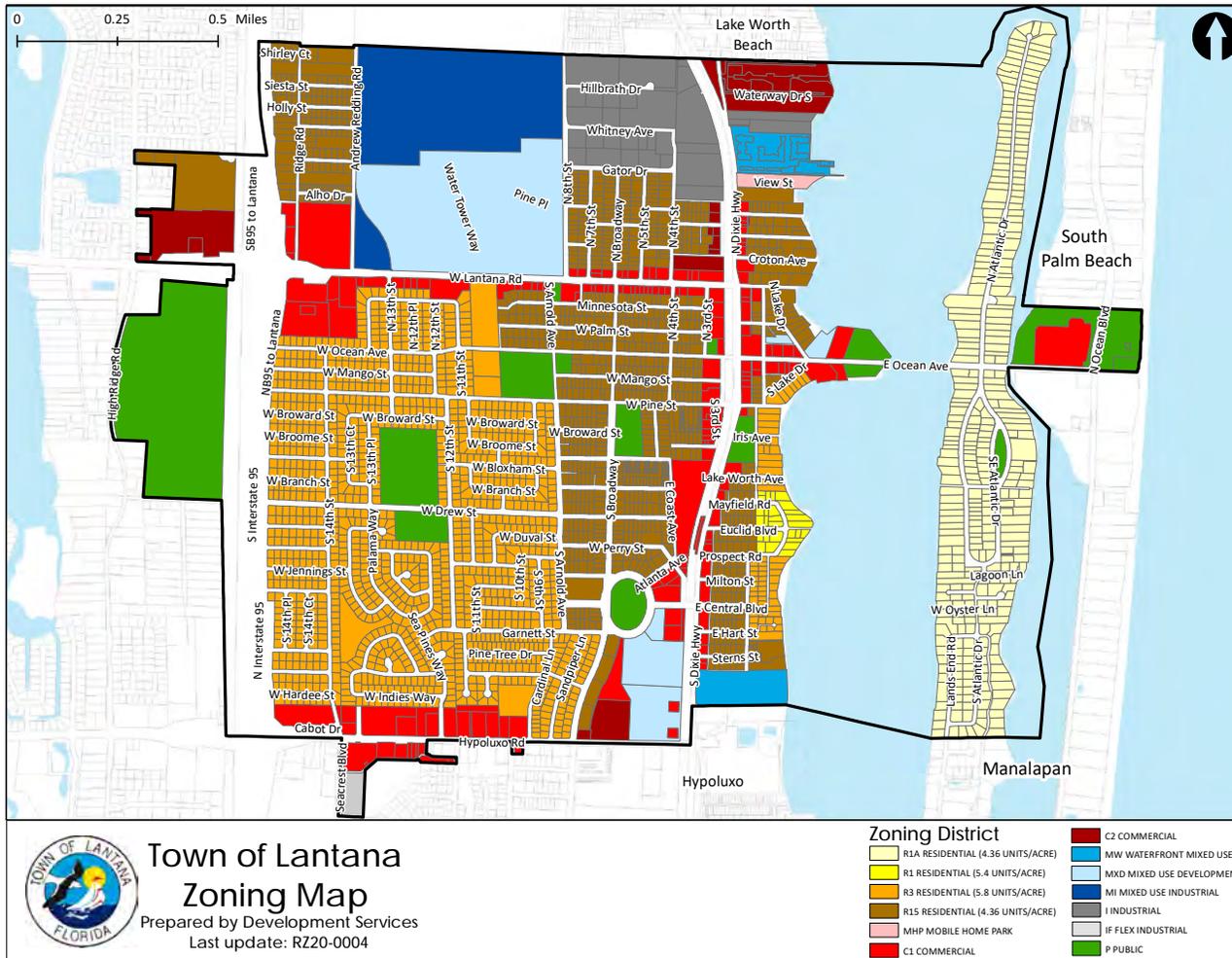
### TOWN IMPROVEMENTS

A Town's entry or monument signs and structures should reflect the community's particular character and the image they want to present to the world. Images of monuments in Palm Beach and Coral Gables, Florida are provided below. Each of these examples are timeless and beautiful in their design and are unique to their place.

The Town of Lantana should pursue specific consistent monument and entry designs at the locations identified on the previous map, with a style that has a historical connection to the Town.



## TOWN IMPROVEMENTS UPDATING THE CODE



## TOWN IMPROVEMENTS

Like many towns and cities, the Town of Lantana is operating with a series of development regulations or codes that have not been updated in many years. The landscape of development and the livability of our coastal downtowns has changed dramatically in the last twenty years so updating the regulations to mirror development expectations and community desires is essential.

Through the charrette and planning process a number of items have been identified that could serve as a start to modernizing the Town's zoning codes.

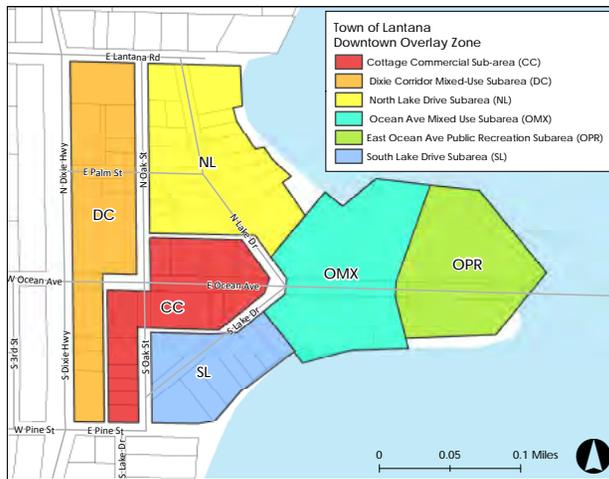
A summary of these recommendations is on the following page with a detailed discussion provided in Chapter III: Key Recommendations.



### TOWN IMPROVEMENTS

#### UPDATING THE CODE

- Redevelopment Incentive Program
- Modifications to the Downtown Overlay Zone (DOZ)
- Parking Revisions - Off-site Possibilities
- Dixie Highway Infill - Zoning Flexibility - Landscape Requirements
- Review Permitted Uses and Special Exceptions Requirements
- Review Site Plan Submittal Requirements - A Tiered Approach
- Possible Historic Preservation Ordinance



The Downtown Overlay Zone map is illustrated at left. This overlay creates six different sub-districts each reflecting the unique existing characteristics within its zone. The overlay rules also provide direction for the creation of a strong urban form and give architectural guidance for new construction.

#### TOWN IMPROVEMENTS

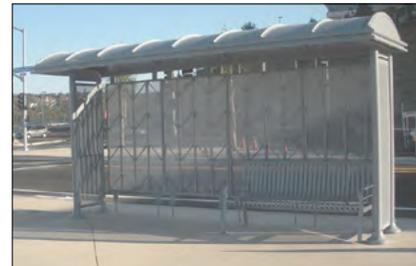
To the left is a list of items to be discussed further in Chapter III: Key Recommendations. These are issues either discussed with the public during the charrette or raised by Town staff throughout the process.

The discussion of these topics in this report alone will not fix the issues, more work in updating the actual codes will need to be done.

However, identifying the current code shortfalls and opportunities to incentivize appropriate redevelopment is an essential starting point.



### TOWN IMPROVEMENTS CONNECTIVITY IMPROVEMENTS



Images representing the US-1 corridor in Lantana.

### TOWN IMPROVEMENTS

In addition to public policy and regulatory improvements, there are some areas within the Town where infrastructure improvements should be prioritized.

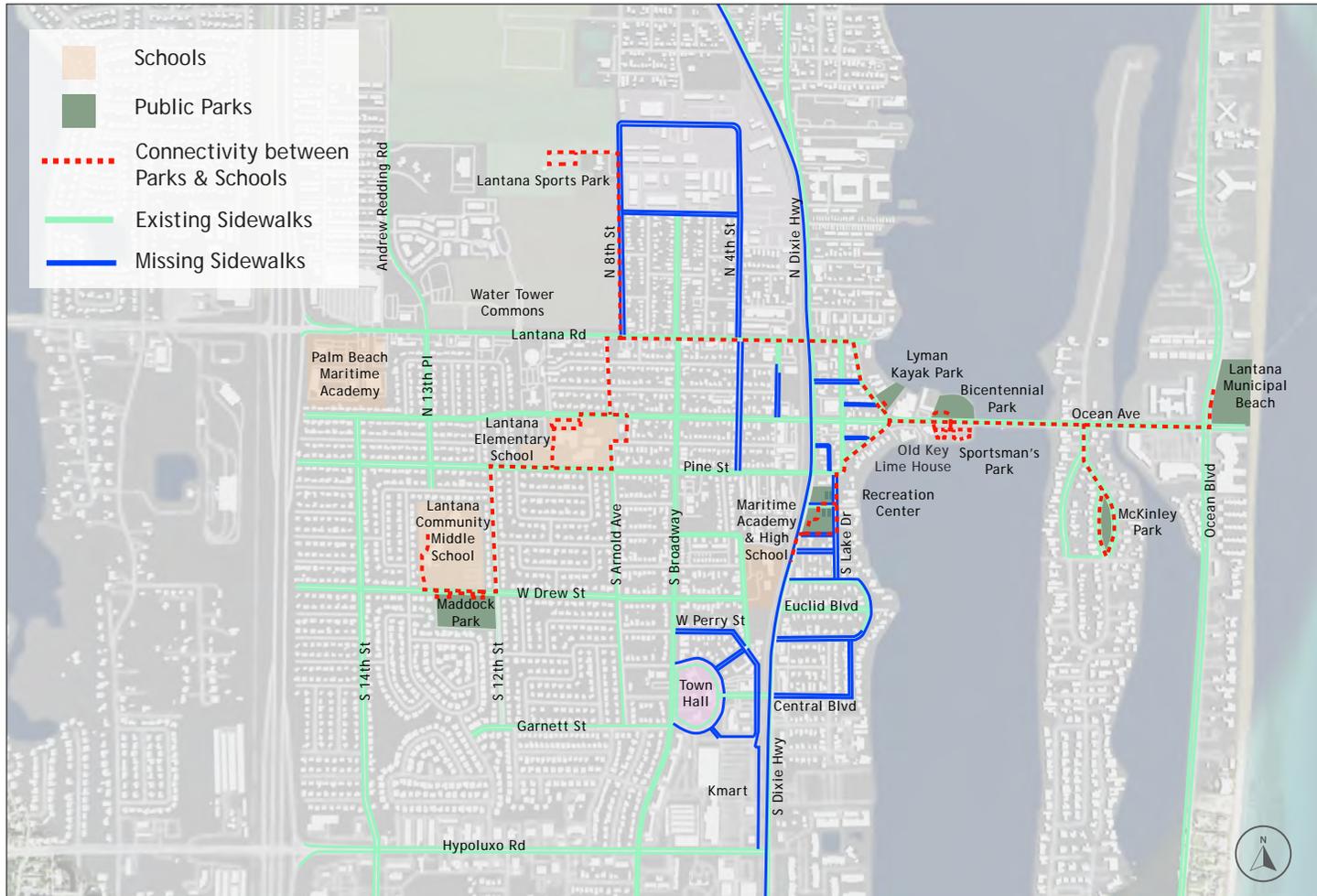
The US-1 corridor in particular is lacking certain basic elements that would provide benefits to the community. The image to the far left illustrates an existing "bus stop" on the west side of the corridor. There are no sidewalks leading to the concrete pad at the stop so transit riders must walk through unimproved conditions to get to their pickup location.

In addition there is no shelter from the elements. The Town, working with the Palm Beach Transportation Planning Agency (TPA) should pursue funding to implement needed improvements.



# TOWN IMPROVEMENTS

## CONNECTIVITY IMPROVEMENTS



This map highlights the missing sidewalks and identifies the connectivity between parks and schools within the Town of Lantana. Additional details and recommendations for improving the connectivity conditions in downtown, Lantana Beach, and Greynolds Circle can be found in Chapter 3.



### TOWN IMPROVEMENTS

#### US-1 REDEVELOPMENT OPPORTUNITIES



*Image of a redevelopment site along US-1 near the intersection of Palm Street.*

#### TOWN IMPROVEMENTS

6

There are a few sites along the US-1 corridor that are obvious redevelopment opportunities. The image at the left is the abandoned commercial site immediately north of Palm Street facing US-1.

The site has a C1 - Neighborhood Commercial zoning designation which does not permit residential. The Market Study that was produced for the master plan states there is very limited retail or commercial growth potential in the near future. This plan recommends that residential be a permitted use through an overlay designation. This overlay would allow residential uses similar to other comparable areas in the county.

### TOWN IMPROVEMENTS

#### US-1 REDEVELOPMENT OPPORTUNITIES



The rendering above illustrates a new townhouse proposal for the US-1 corridor site illustrated on page 71. This design proposes nine three-story townhouses with rear detached garages and accessory dwelling units above. Each of the units has a private courtyard with the potential for small private swimming pools. This proposal yields a residential density of 30 dwelling units per acre. The approval of residential on commercial sites should be contingent upon the design principles illustrated in this plan.

### TOWN IMPROVEMENTS

#### US-1 REDEVELOPMENT OPPORTUNITIES



*The street-view rendering above illustrates the townhouse design proposal on page 72. The architectural style of new redevelopment should reflect the design spirit of the community and can be varied from place to place. What should not be negotiable are the design principles of new buildings facing the streets with wide sidewalks, shade trees, activated ground floor levels, and parking to the side, rear, or on-street.*

### TOWN IMPROVEMENTS

#### US-1 REDEVELOPMENT OPPORTUNITIES



*Magnolia Court Townhouses, US-1 West Palm Beach, Florida*



*Town Center Townhouses, US-1 Delray Beach, Florida*

#### TOWN IMPROVEMENTS

6

There has been a growing trend in the last decade of commercial parcels along the US-1 corridor being converted to residential and/or mixed-use projects. This is in part due to the evolving nature of the corridor itself: from a commuter-oriented facility to a more neighborhood and community-serving facility.

The images to the left illustrate just two of the many successful townhouse developments along the US-1 corridor. These projects can also be found in Lake Worth Beach, Boynton Beach, and many other areas of the county.

This plan recommends the Town enable this type of redevelopment consistent with the planning principles outlined in the Downtown Overlay.



## KEY RECOMMENDATIONS & IMPLEMENTATION

The success of this (and any other) Master Plan will depend on its ability to be implemented economically and socially within a designated time frame. To that end, the recommendations throughout this report have been developed as independent but interrelated projects. Some, such as public infrastructure projects, are within the Town's control to pursue implementation, with funding and current staff workload being the primary challenge. Other recommendations are redevelopment techniques that are illustrated on private property - which are subject to each private entity's time frame and financial situation. In order to realize these types of projects, the principles of urban design described and illustrated throughout the report and in the examples provided have to be embedded culturally within the Town, required by its codes, and encouraged through its programs. The combination of public and private efforts is required for realizing the vision of the Master Plan.

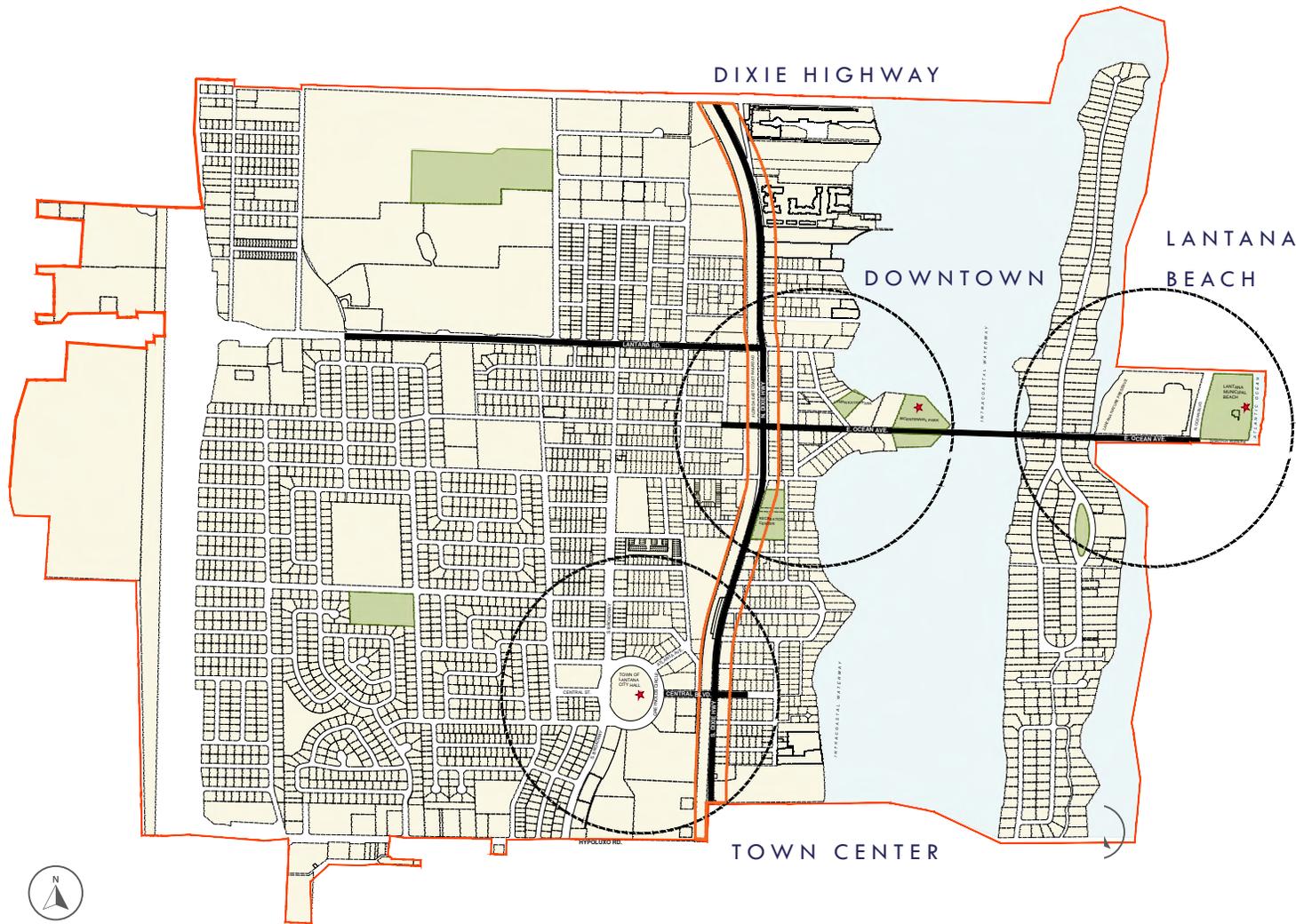
## LAND DEVELOPMENT REGULATIONS

Updating the existing Land Development Regulations (LDRs) will be important in ensuring that the community vision is more predictably attainable. During the public design charrette a number of code elements were identified as needing attention. LDRs are intended to both protect existing residents and businesses from negative impacts of adjacent development and to ensure a desirable physical form. LDRs can also serve as a redevelopment tool - providing critical information to potential investors and a clear approval process. If it is difficult to ascertain what can be built and how long the process takes, codes can function as a disincentive for redevelopment.

During the design charrette and in meetings with staff the items on the following pages were identified for potential code revisions.



**FIGURE 10** TOWN OF LANTANA DISTRICT MAP



## INTRODUCTION

In order to organize proposed improvements and to focus those efforts in the most critical areas, the plan has identified three distinct districts within the town. The map to the left has identified the Lantana special districts:

- The Downtown District
- The Town Center District
- Lantana Beach District

The following pages will go into greater detail as to specific improvements and policy changes that should be considered for each district.



DOWNTOWN

**DESCRIPTION**  
The Downtown District is an area within a 1/4 mile radius measured from the 200 block of E Ocean Avenue and includes the Downtown Dining Incentive area, Bicentennial Park, N Dixie Highway and areas just west of the FEC corridor including the newly renovated Lantana Library.

The intent of identifying the Downtown District is to focus specific improvements unique to the Town's historic commercial heart. Just beyond the iconic main street condition of E Ocean Avenue exists some of the older neighborhoods of the town. Striking a balance between commercial demands and protecting the neighborhoods is essential in this area. Parking is an issue and some off-site solutions to better define additional parking without intruding into the neighborhoods are provided on the following pages.

Within the Downtown District a special subarea has been defined as the Downtown Dining Incentive area which focuses specific incentives to assist with invigorating the commercial heart of the town.

FIGURE 11 DOWNTOWN DISTRICT



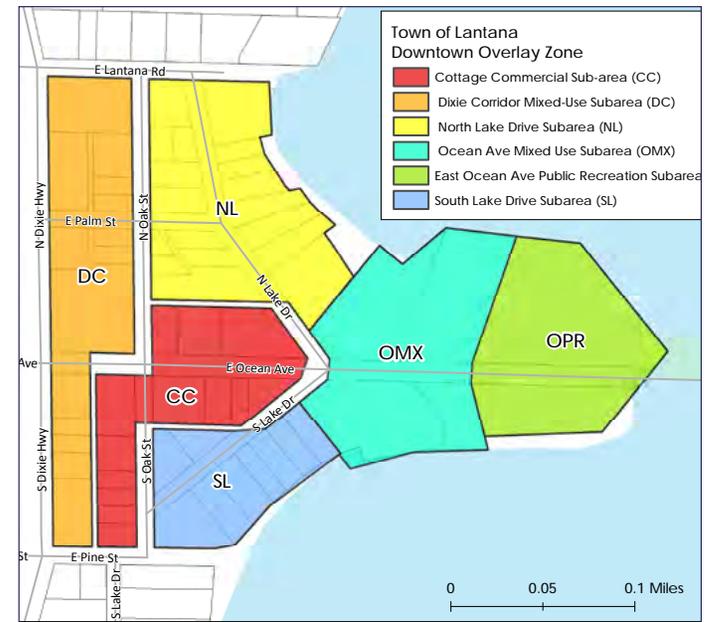
## DOWNTOWN

### DOWNTOWN OVERLAY ZONE RECOMMENDATIONS

The existing Downtown Overlay Zone (depicted at right) is a solid approach to defining specific regulations and expectations for redevelopment within the six identified sub-districts. The following are recommendations for improvements:

- Permit up-to three story buildings within the Cottage Commercial Sub-area as a limited duration incentive program (to encourage redevelopment)
- Eliminate the required building step back above the second floor
- Establish criteria for what constitutes "contributing structures of historic architectural character" within the sub-areas
- The criteria for contributing structures within the sub-areas should be based upon the urban design elements in the Downtown Overlay Zone
- Identify which structures within the sub-areas could be considered contributing based upon the identified criteria
- Relax non-conformity compliance for existing contributing structures within the sub-areas (to encourage reuse and occupancy)
- Use flexible non-conformity compliance to encourage provision of the most critical features for the downtown
- Relax on-site parking requirements for existing contributing structures within the sub-areas
- Provide for off-site parking strategies for all existing and proposed structures within the sub-areas
- Follow the building height and streetscape recommendations in this section.

**FIGURE 12** TOWN OF LANTANA DOWNTOWN OVERLAY ZONE



DOWNTOWN

DOWNTOWN DINING INCENTIVE AREA

To incentivize development in downtown, the downtown dining incentive area as defined in the figure below focuses the incentives to the core of the downtown district. Town owned land to the west of Dixie Highway is underutilized for parking, and recommendations for improvements are made on the following pages. A parking exemption program could apply to the new downtown dining district to motivate redevelopment and encourage new businesses.

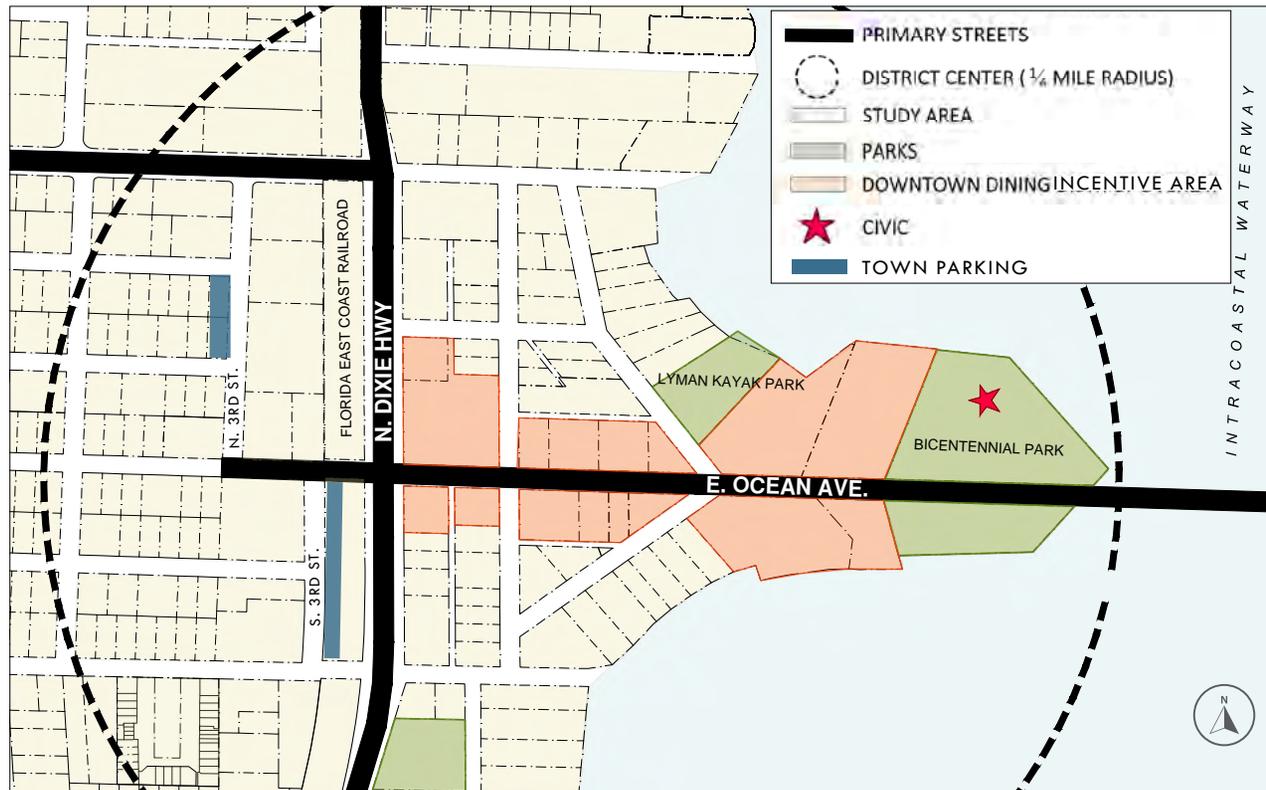


FIGURE 13 DOWNTOWN DINING INCENTIVE AREA

WAYFINDING SIGNAGE

Providing ample, close, and safe parking areas off-site for the downtown businesses is an important element to fostering new investment. Clear and easily understandable way finding signage will be important to direct downtown patrons to newly defined parking areas.



## DOWNTOWN

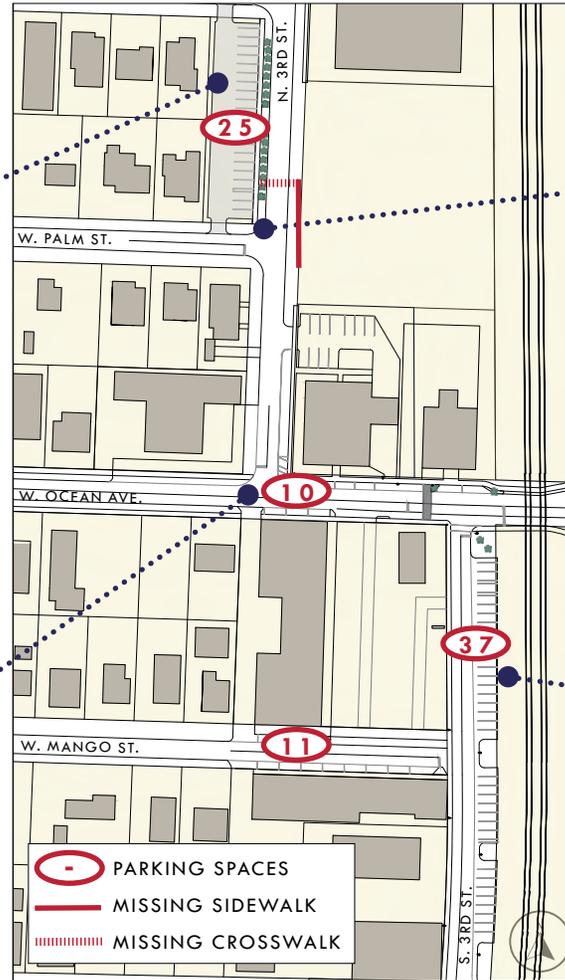
### EXISTING PARKING & STREETScape AT W OCEAN AVE & 3RD ST



There is very little information communicating this parking as available for the community. A town sign with the number of spaces available and the distance to the downtown dining district should be added.



There are many on-street parking spaces which are no longer striped. For downtown visitors these spaces are very hard to see and unclear if they are available.



Complete sidewalk from the 3rd Street Parking lot with a mid-block crossing to the sidewalk located on the east side of 3rd Street.



These spaces may be underutilized and pose safety issue since there is no sidewalk. Users are pushed into the FEC Railway or into the vehicular travel lanes before they reach a sidewalk.



## DOWNTOWN

### PARKING & STREETScape AT OCEAN AVE & 3RD ST RECOMMENDED IMPROVEMENTS



Add masonry wall with mural to the west edge of the parking lot to buffer the neighborhood from the parking area. Mural could feature native flowers. This helps to communicate the parking lot as a community amenity. "Vermont Flowers," Kathryn Wiegels



Mark the transition to the neighborhood to help prevent neighborhood intrusion of downtown visitors. Added shade trees is one way of defining the neighborhood. - Lake Street north of Belvedere Road in WPB



Utilize pervious materials in on-street parking to help mitigate stormwater impacts.

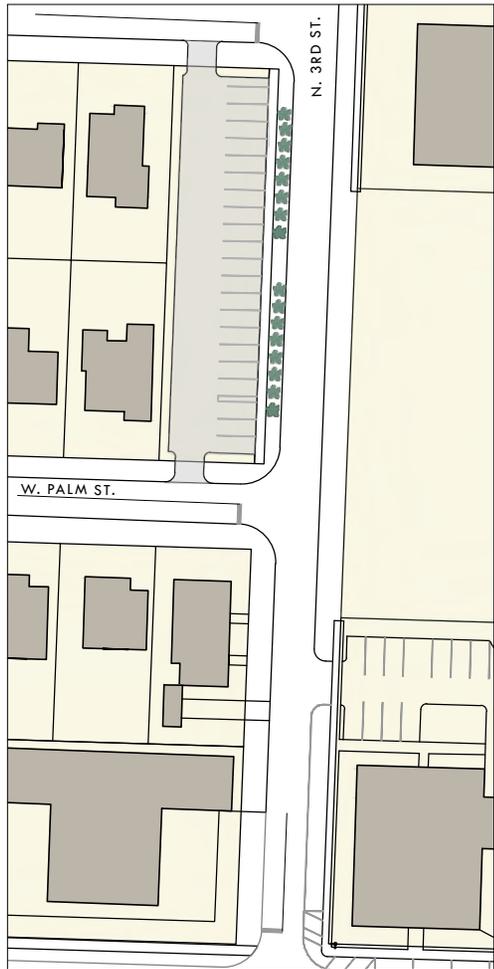


Define the edges of on-street parking. This adds opportunities for low-impact development for stormwater and green beautification. These small islands occur where the sight lines would not have allowed for additional parking. They also provide or safe refuge for pedestrians.



## DOWNTOWN

### PARKING & STREETScape AT 3RD STREET PARKING LOT



Detailed view of the existing 3rd Street parking lot and streetscape.

The image above shows the existing conditions at the town parking lot which is less than an eighth of a mile (1/8) away from the downtown dining district incentive area. The lot has very little markers to indicate this is available town parking.

## DOWNTOWN

### PARKING & STREETScape AT 3RD STREET PARKING LOT

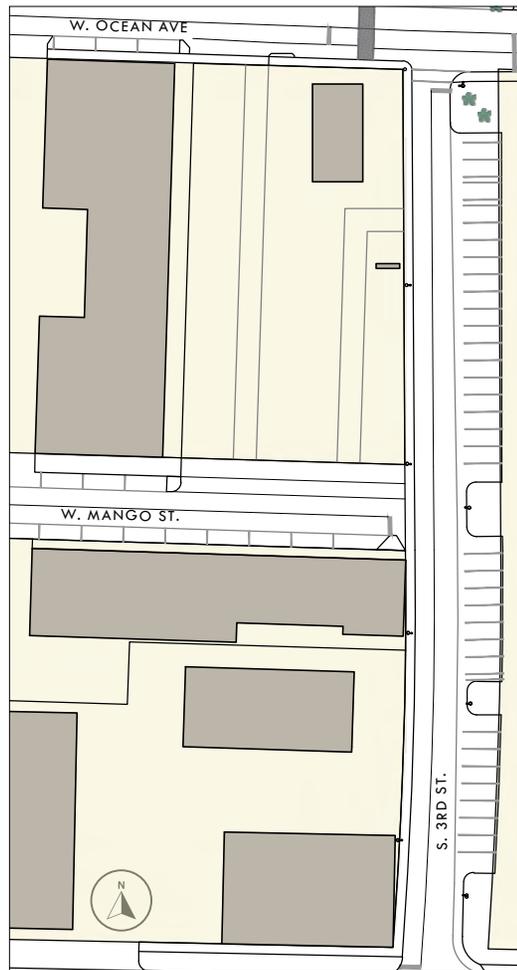


Detailed view of the improved 3rd Street parking lot and streetscape.

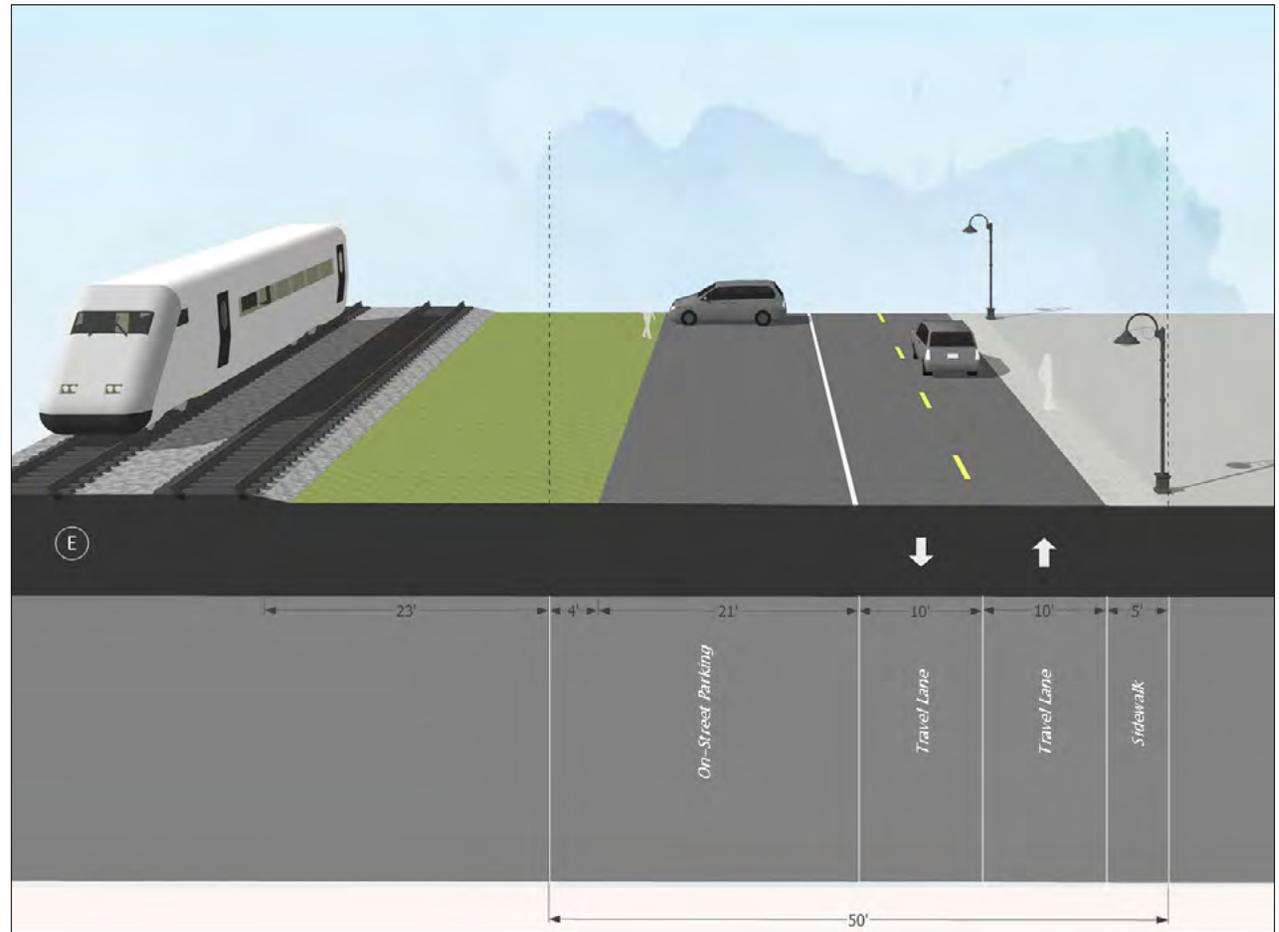
The image above illustrates the 3rd Street Parking lot with the addition of signage, native landscaping, pedestrian lighting, and a new masonry wall with a Coreopsis flower mural. These interventions help to beautify but also communicate available parking. The improvements to the area will also project a sense of safety and security which will also benefit the Lantana Public Library to the south.

## DOWNTOWN

### PARKING & STREETScape AT 3RD STREET



Detailed view of the existing on-street parking along S 3rd Street and W Mango Street.

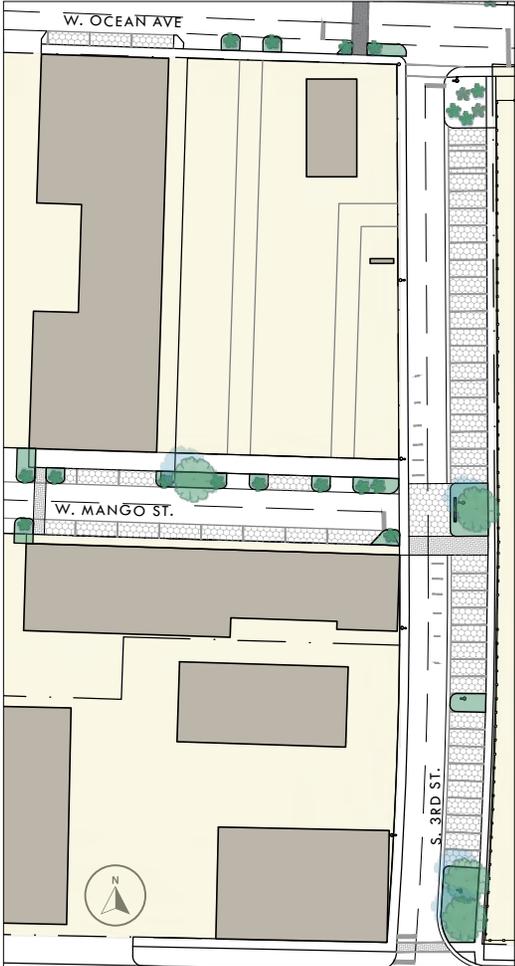


The image above is looking south and illustrates the existing conditions at the S 3rd Street parking parallel to the FEC Railway. The parking on the east, the travel lanes, and the sidewalk on the west are all within the right-of-way. There is an opportunity to improve this area to formalize the spaces and make the function of these spaces safer.



DOWNTOWN

PARKING & STREETScape AT 3RD STREET



Detailed view of the improved on-street parking along S 3rd Street and W Mango Street.



In the concept above the existing sidewalk to the west and the FEC Railway including the 25' clear zone remain the same. Within the Town's right of way a new fence, sidewalk, curb and gutter are added to improve safety along the FEC Railway. The concept also illustrates an opportunity to add pervious pavers to the on-street parking with an enhanced mid-block crossing.



DOWNTOWN

MICRO-TRANSIT

A number of cities and towns within the region are augmenting existing municipal and/or county public transit with independent micro-transit operators. These have proven to be a successful addition to in-town mobility and should be considered for the Town of Lantana's Downtown District.



Circuit in Pompano Beach



Freebee in Delray Beach

PRIMARY STREETS & ENTRANCES

The District Maps identified in this section of the report include designations of Primary Streets within the districts. These are typically the most prominent and public of corridors and it is important that the fronts of buildings, and public entrances, be provided along these roadways. The recently renovated Lantana Public Library is currently not permitting access from Ocean Avenue. This should be reconsidered.



"Not an Entrance" at the new Library fronting Ocean Ave.

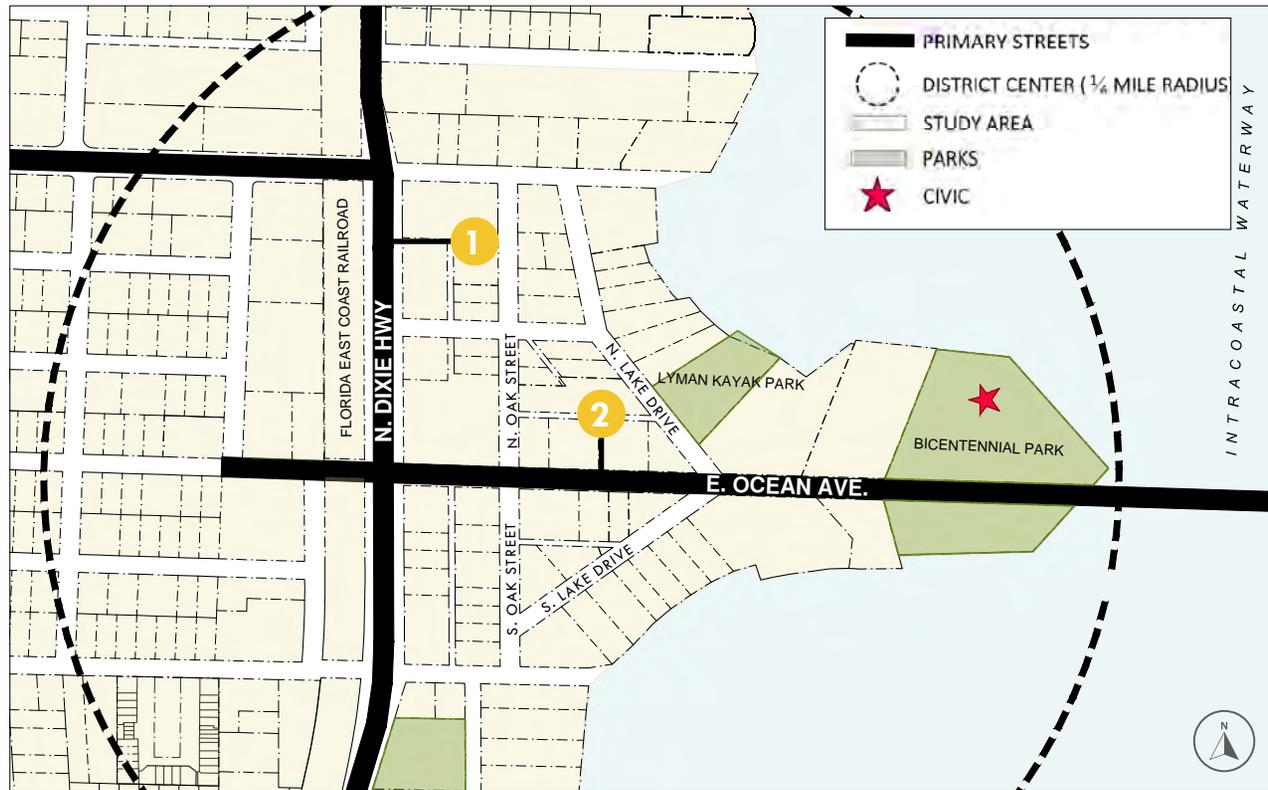


Library from Ocean Ave has attractive and prominent entrance.



## DOWNTOWN STREETSCAPE

The design team developed specific streetscape configurations for two locations within the Downtown Overlay Zone (DOZ). Those locations are at N Dixie Highway and East Ocean Avenue and are identified on the key map below. The sections are on the following page.



**FIGURE 14** DOWNTOWN DISTRICT

### N. DIXIE HWY. SECTION

1

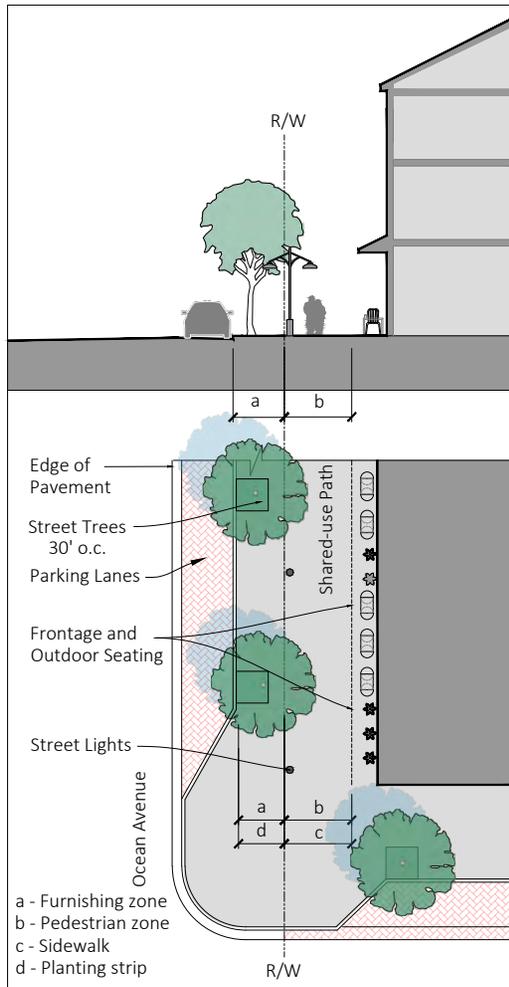


### E. OCEAN AVE. SECTION

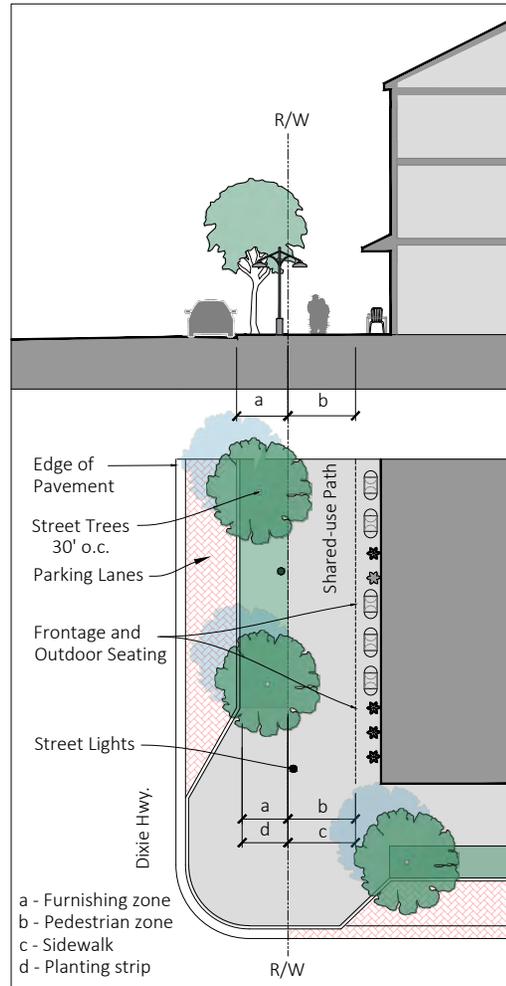
2



## DOWNTOWN



**FIGURE 15** E. OCEAN AVENUE STREETScape



**FIGURE 16** N. DIXIE HIGHWAY STREETScape

### E. OCEAN AVE. SECTION

1

The East Ocean Avenue section includes the existing on-street parking, a furnishing zone for landscaping and infrastructure and a pedestrian zone wide enough for dining and pedestrian activity (typically +15'). The increase in sidewalk and streetscape depth would occur through redevelopment.

### N. DIXIE HWY. SECTION

2

The North Dixie Highway section is developed specifically for the residential/mixed-use townhouse typologies illustrated earlier in the report. These illustrate a deep furnishing zone, a landscape parkway wide enough to accommodate shade trees, and a deep pedestrian zone for a total minimum of 30', giving a bit of a buffer between the road and the residences.



TOWN CENTER

**DESCRIPTION**

The Town of Lantana Town Center District is identified as the 1/4 mile radius centered about the municipal campus at Greynolds Circle. This area includes portions of the neighborhoods and extends eastward to include portions of S. Dixie Highway.

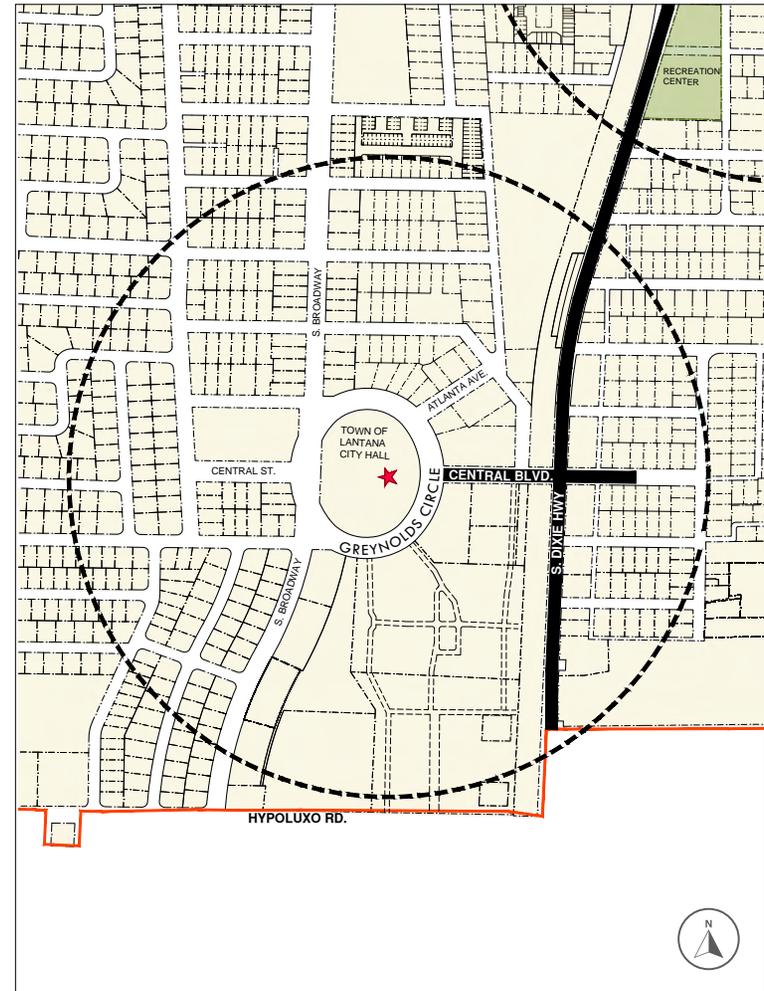
The Town Center District is intended to focus physical and policy improvements within the municipal complex and adjacent neighborhoods.

The recommendations provided herein look at streetscape and tree planting improvements, a reaffirmation of Greynolds Circle remaining the municipal campus for the future, and redevelopment policy direction for the Kmart/Winn Dixie plaza.

The following pages provide greater detail on specific projects for the Town Center District.



FIGURE 17 TOWN CENTER DISTRICT

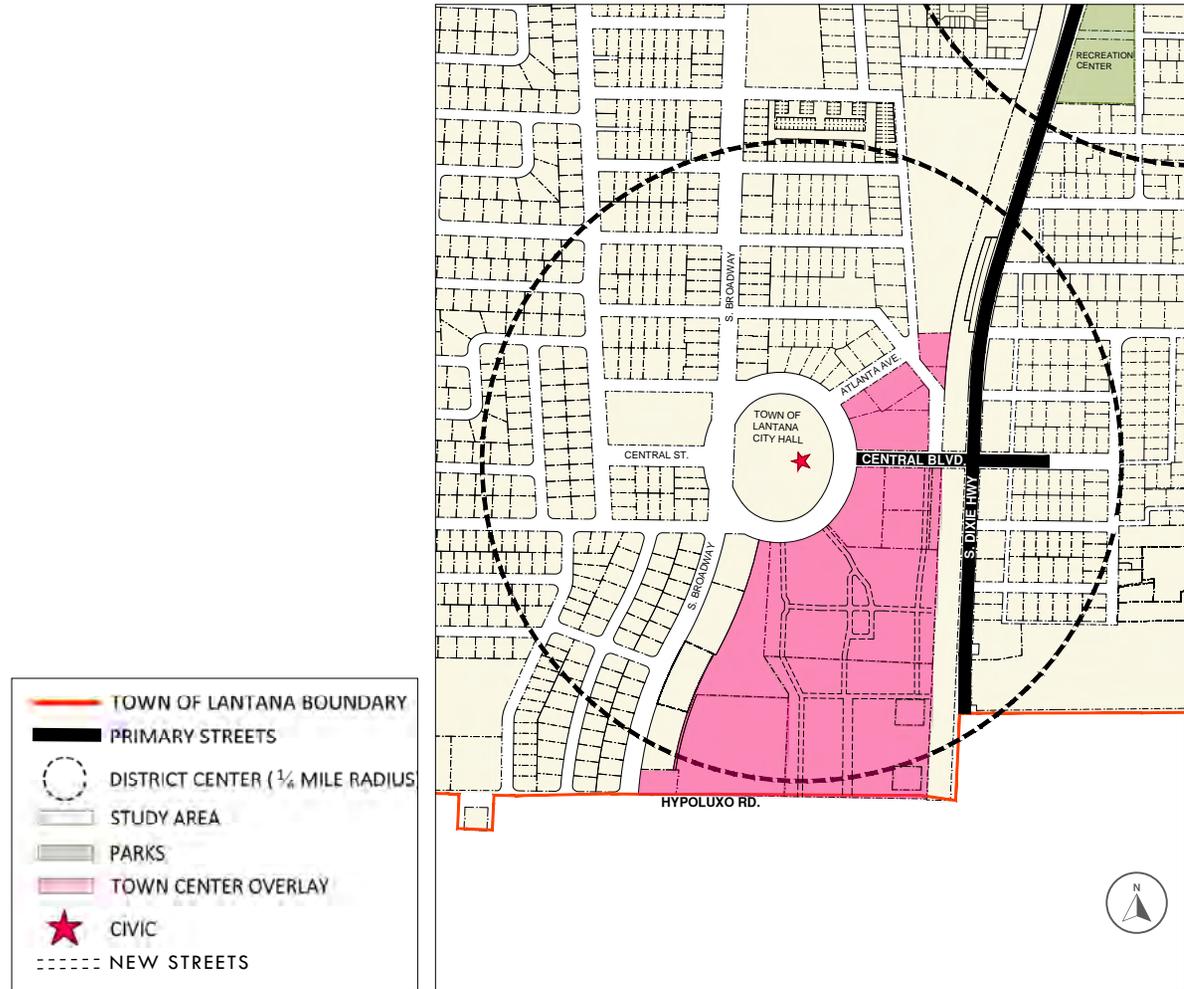


## CODE RECOMMENDATIONS

### TOWN CENTER REDEVELOPMENT OVERLAY

There will be the rare occasion in the Town of Lantana whereby a larger scaled redevelopment opportunity presents itself. The Winn Dixie/ Kmart site south of Greynolds Circle is a prime example of this unique and challenging prospect. In an effort to provide the greatest predictability and built environment for these future projects, this plan recommends the Town consider creation of the Town Center Redevelopment Overlay zoning option. The recommendations to follow are specific to this new zoning opportunity.

**FIGURE 18** TOWN CENTER REDEVELOPMENT OVERLAY



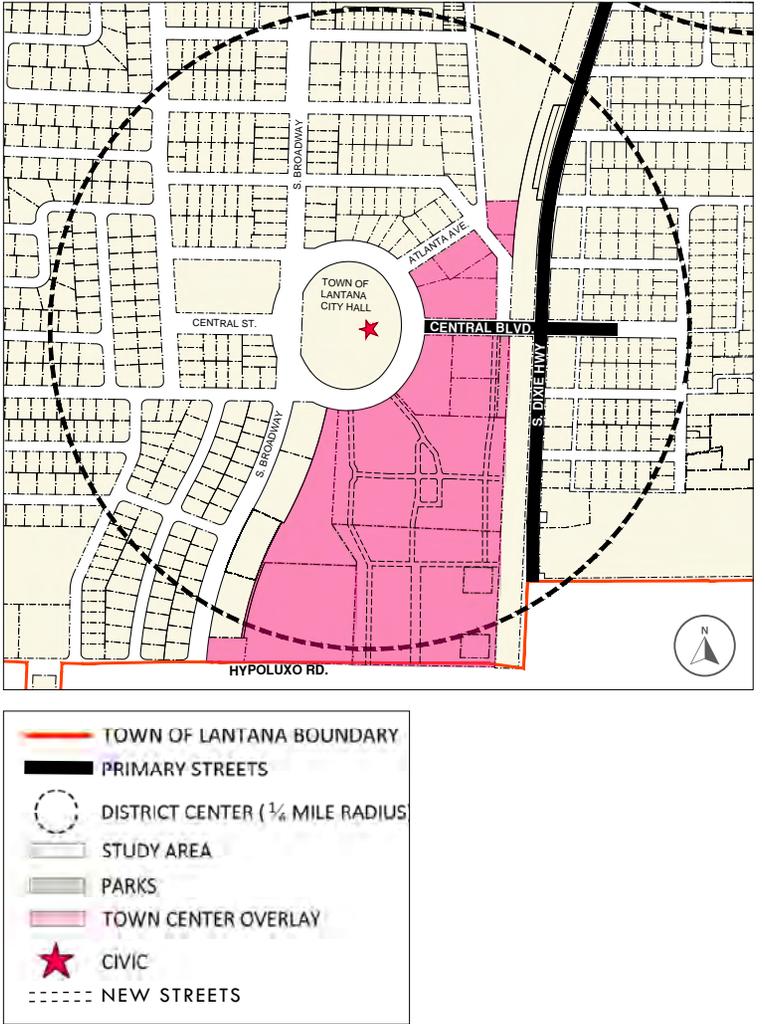
CODE RECOMMENDATIONS

TOWN CENTER REDEVELOPMENT OVERLAY

- Applies only to the MXD, C-1 and C-2 zoning districts.
- The development site must be a minimum of three contiguous acres.
- Allows for up-to 25 dwelling units per acre residential.
- Allows for up-to five stories as measured in Figure 34 on page 113.
- Requires a minimum 10% of all ground floor space be non-residential uses (retail, commercial, office, Live-Work space, flex space).
- Requires a minimum 10% overall site area be designed and developed as Civic Open Space as defined in Figure 35 on page 115.
- Requires compliance with the Street and Block Standards as defined on page 118.
- Complies with the Downtown Overlay building frontage percentage requirements.
- Complies with the Streetscape standards beginning on page 116.
- Complies with the parking and access requirements of the Downtown Overlay and parking diagram recommendations in Figure 37 on page 117.
- Complies with the Frontage Type standards beginning on page 123.
- Requires an overall master plan for the site regardless of size and ownership structure.

See the **"KMart/Winn Dixie Site"** provided [Chapter II: Tour of the Plan](#) for examples of how this overlay could be developed.

FIGURE 19 TOWN CENTER REDEVELOPMENT OVERLAY



TOWN CENTER

GREYNOLDS CIRCLE CONNECTIVITY IMPROVEMENTS

The plan below identifies 6 special project areas around the municipal campus and at Greynolds Circle. Each of these areas contemplates new tree plantings, transit shelters, and sidewalk connections and crosswalks. These connectivity improvements should be expanded over time to provide physical improvements throughout the Town with a focus on connecting parks and schools.

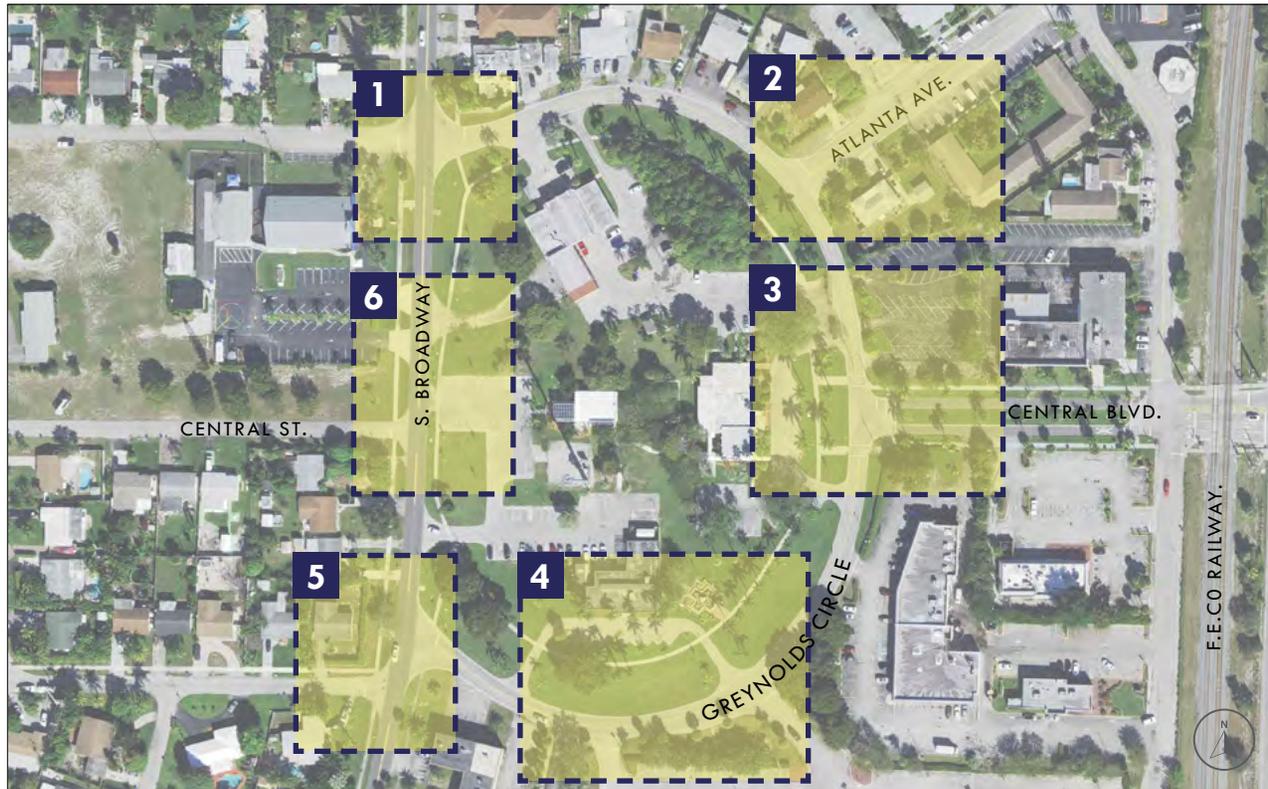


FIGURE 20 GREYNOLDS CIRCLE IMPROVEMENT KEY

CONNECTIVITY IMPROVEMENTS

CANOPY TREES

TRANSIT SHELTERS

SAFE CROSSINGS



## TOWN CENTER



**FIGURE 21** GREYNOLDS AND BROADWAY IMPROVEMENT DIAGRAM

### CONNECTIVITY IMPROVEMENTS

1

#### GREYNOLDS AND BROADWAY

- Remove concrete and add native landscape or sod along Greynolds
- Improve Transit stops with shelters
- Complete the sidewalk connections at the intersection of Greynolds and S. Broadway



## TOWN CENTER



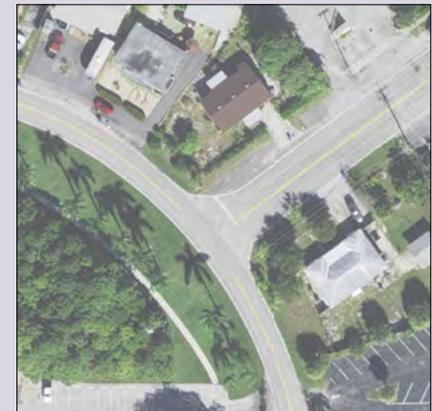
**FIGURE 22** GREYNOLDS AND ATLANTA IMPROVEMENT DIAGRAM

### CONNECTIVITY IMPROVEMENTS

2

#### ATLANTA AVE.

- Remove concrete and add native landscape or sod along Atlanta Ave
- Define on-street parking.
- Complete the sidewalk from Atlanta Ave. to connect to Greynolds Circle with new crosswalks
- Add shade trees



## TOWN CENTER



**FIGURE 23** GREYNOLDS AND CENTRAL IMPROVEMENT DIAGRAM

### CONNECTIVITY IMPROVEMENTS

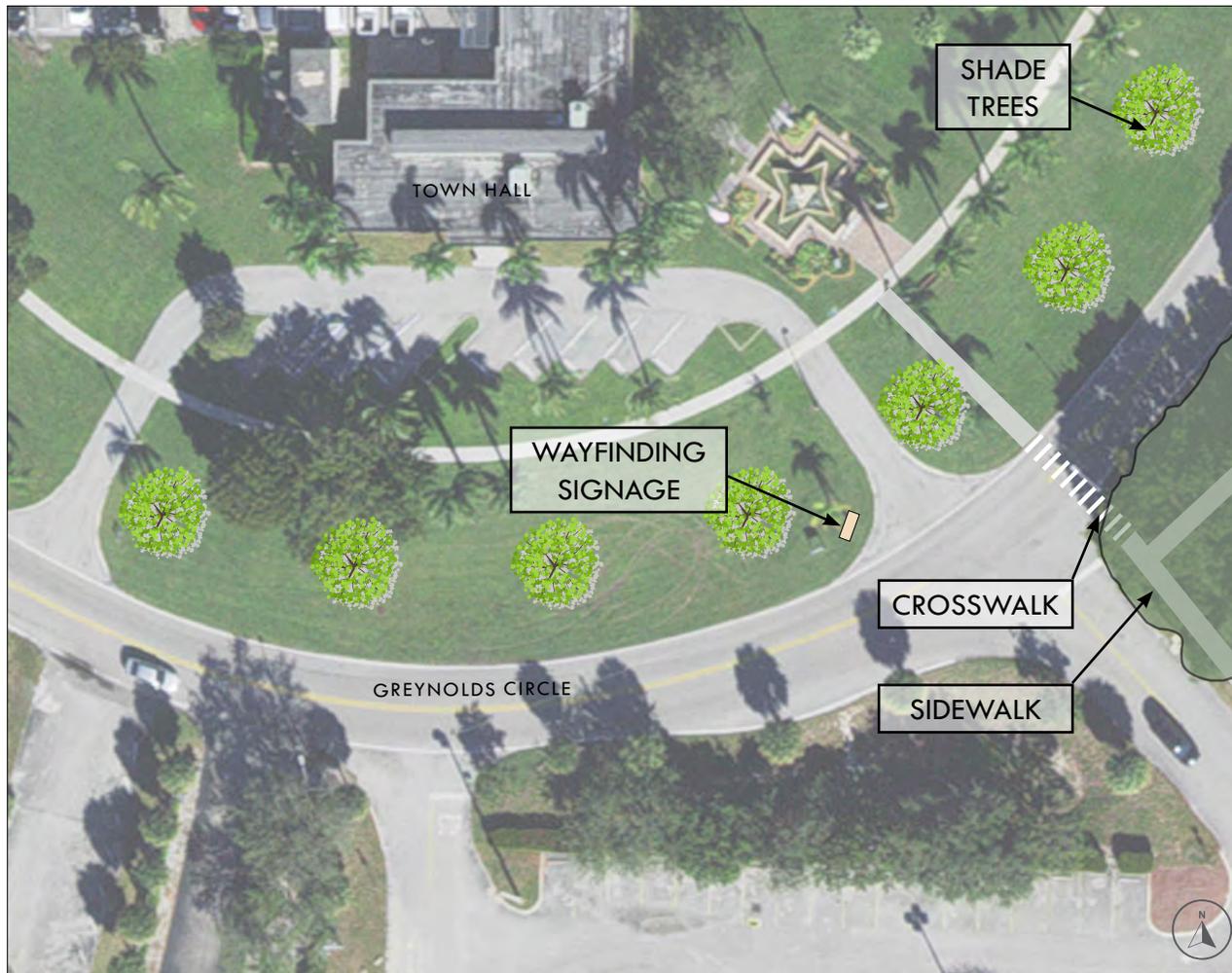
3

#### CENTRAL BLVD.

- Remove asphalt defining the entrance to the municipal campus and creating larger soil volume for specimen tree
- Complete the sidewalk along the east side of Greynolds Circle connecting Central Blvd.
- Add shade trees



## TOWN CENTER



**FIGURE 24** SOUTH GREYNOLDS CIRCLE IMPROVEMENT DIAGRAM

### CONNECTIVITY IMPROVEMENTS

4

#### SOUTH GREYNOLDS

- Connect existing sidewalk network and add a crosswalk to the south
- Add shade trees
- Enhance existing wayfinding at the entrance to the Town Hall



TOWN CENTER

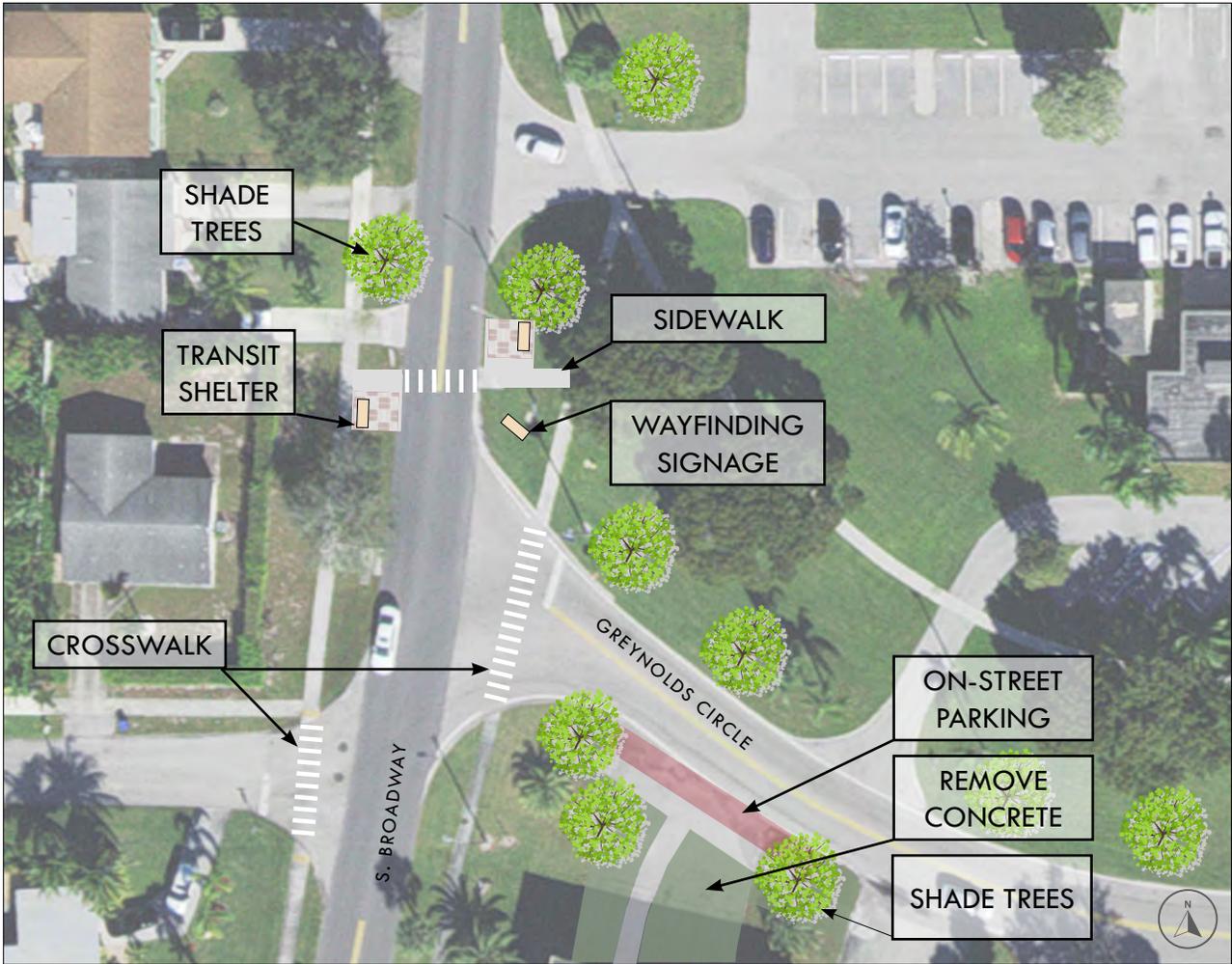


FIGURE 25 GREYNOLDS AND BROADWAY IMPROVEMENT DIAGRAM

CONNECTIVITY IMPROVEMENTS

5

GREYNOLDS AND BROADWAY

- Removal of concrete currently utilized for 4 parking spaces and replace with parallel parking, sidewalk, and landscape improvements
- Relocate transit stop on the west side of Greynolds Circle and improve both transit stops with shelters and connected sidewalks
- Add shade trees



## TOWN CENTER



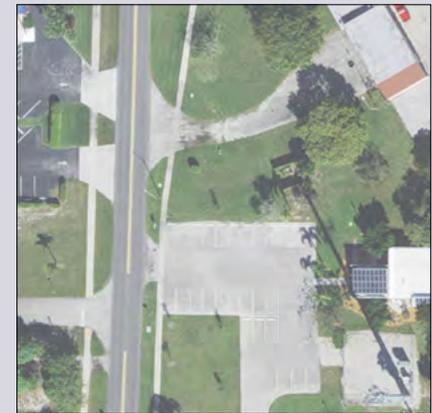
**FIGURE 26** GREYNOLDS AND BROADWAY SOUTH IMPROVEMENT DIAGRAM

### CONNECTIVITY IMPROVEMENTS

6

#### BROADWAY

- New pedestrian connection from West Central across Greynolds Circle
- Add shade trees
- Add wayfinding at the terminus of West Central



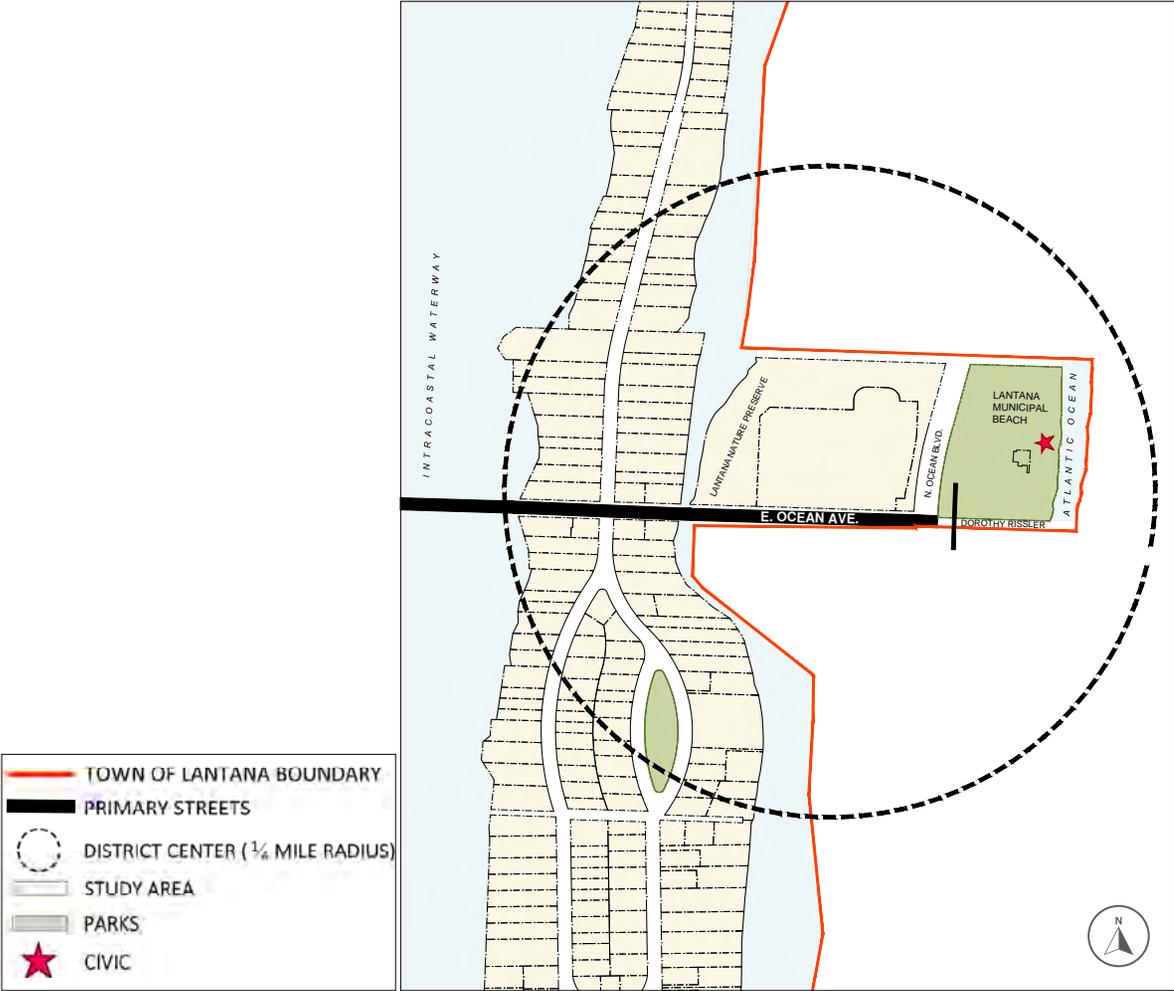
# LANTANA BEACH

**DESCRIPTION**  
 The Town of Lantana Beach District is defined as a 1/4 mile radius centered on E. Ocean Avenue immediately west of the municipal beach proper and is illustrated to the right.

During the course of the public design charrette there was much discussion about what and how to improve the Lantana Municipal Beach asset. Many larger scale, longer-term design ideas are provided in Chapter II of this report however, Town staff requested more specific immediate improvements that could be implemented.

The following pages reflect detailed observations and recommendations developed during field work at the Lantana Beach. These recommendations should be prioritized and become part of a multi-departmental improvement program.

FIGURE 27 LANTANA BEACH DISTRICT



LANTANA BEACH

INCREMENTAL IMPROVEMENTS

The following pages describe a number of incremental improvements that could be made over time to enhance the quality of the environment and visitor experience at the Lantana Public Beach.

Improvements recommended include:

- New decking and railing
- Rebuilt beach access
- Landscaping and parking lot improvements
- Suggestions for the existing pavilion

The following pages illustrate existing conditions and examples of recommended improvements from other similar facilities. Many municipal beaches have been improved over the years so there are multiple examples of lessons learned throughout the region.



LANTANA BEACH

One of the first obvious improvements needed at Lantana Beach is the replacement of much of the decking and railings. There are some conditions which appear unsafe and need immediate attention. This is contrasted by the amazing destination of the Dune Deck Cafe restaurant (pictured below) which is very popular in the area.

The upgrading of the decking and railings leading to the beach access and Dune Deck Cafe should be top priority for improvements.



EXISTING



RECOMMENDED

- Rebuild beach access
- Continue the Town's theme and colors with branding throughout the improvements



LANTANA BEACH

EXISTING



RECOMMENDED



- Remove sod from parking islands
- Replace with native grasses/plantings
- Resurface parking lot with pervious pavement

- Stabilize picnic areas with concrete platforms
- Pavilions as needed for shade
- Improve connectivity with pathways

- Define a perimeter for the playground
- Screen the parking areas
- Add shade structures or shade sails



LANTANA BEACH

EXISTING



RECOMMENDED



- Create a pedestrian pathway system connecting the different areas of the property
- Add native landscaping
- Implement a branded wayfinding system and connective pathway network throughout the property
- Install structured bicycle parking
- Consider adding bike rental facility connecting downtown to the beach

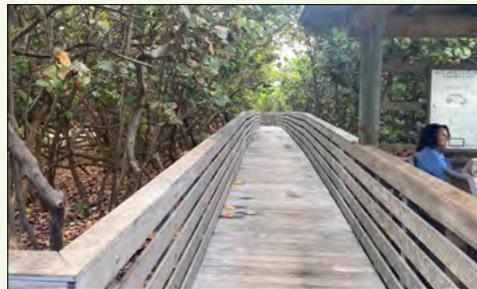


## LANTANA BEACH

### EXISTING



### RECOMMENDED



- Remove invasive Brazilian Pepper
- Continue to maintain dune stabilizing native landscaping

- Remove retired pedestrian lighting and continue to replace with Dark Sky friendly lighting

- Replace decking and railings

LANTANA BEACH

Reprogram or remove the Ocean Front Pavilion

The existing beach pavilion at Lantana Beach should be inspected for any mechanical and/or structural improvements that might be needed to restore it as a functioning asset. There are clearly some exterior facade and roof fascia work needed however it would be beneficial for the Town to understand the estimated expense to restore the building. Having this information prior to continuing a public discuss on what to do with the pavilion would frame those discussions in a more realistic and productive way.



LANTANA BEACH



FIGURE 28 DOROTHY RISSLER DR. EXISTING CONDITIONS

DOROTHY RISSLER DR. IMPROVEMENTS

One of the primary entrances to the Lantana Public Beach is from Dorothy Rissler Dr. which is effectively the extension of E. Ocean Avenue from S. Ocean Boulevard to the beach. Dorothy Rissler Dr. also separates the beach property from the Eau Palm Beach Resort and Spa to the south and serves as the "back of house" to that facility.

The image to the left illustrates delivery and service access to the Eau and beach access to the left.



## LANTANA BEACH



**FIGURE 29** DOROTHY RISSLER DR. PROPOSED IMPROVEMENTS

### DOROTHY RISSLER DR. IMPROVEMENTS

The design team explored ways to improve the aesthetic environment of Dorothy Rissler Dr. without hindering its functionality to the beach and the Eau.

Improvements (illustrated at left) include decorative paving treatments to emphasize the beach entry and better define the public realm from the service realm of the Eau. Additional landscaping towards the beach entry seeks to soften that edge and focus attention on the beach side.



## DIXIE HIGHWAY CORRIDOR IMPROVEMENTS

#### DESCRIPTION

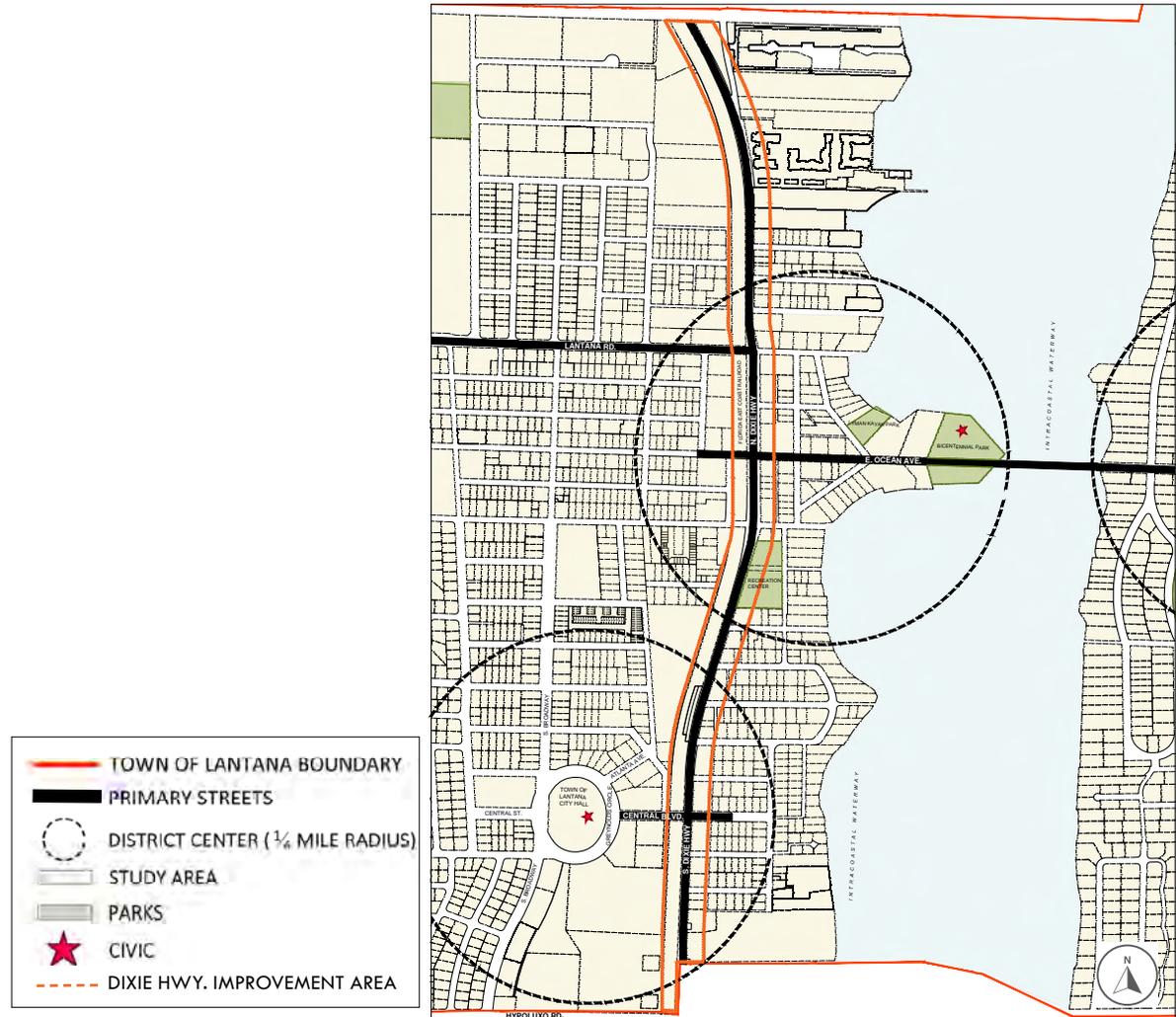
The Dixie Highway Corridor improvements include areas inside and outside of the previously described Downtown, Town Center, and Lantana Beach Districts and is highlighted in the image to the right.

The Dixie Highway corridor in Lantana provides opportunities for improvement and redevelopment. Some design concepts for redevelopment have already been illustrated in Chapter II of this plan. Identifying infill redevelopment opportunities along the corridor as well as public works type of improvements (sidewalks, crosswalks, landscaping, shade shelters, etc) should be prioritized.

The following pages discuss some recommended code modifications as well as a cross-section locating new physical improvements.

Many of the historic coastal communities in Palm Beach County have encouraged mixed-use and townhouse redevelopment of the corridor which should be reviewed as to their appropriateness to the Town of Lantana.

**FIGURE 30** DIXIE HIGHWAY CORRIDOR IMPROVEMENTS



## DIXIE HIGHWAY CORRIDOR IMPROVEMENTS

### DIXIE HIGHWAY CORRIDOR INFILL OVERLAY

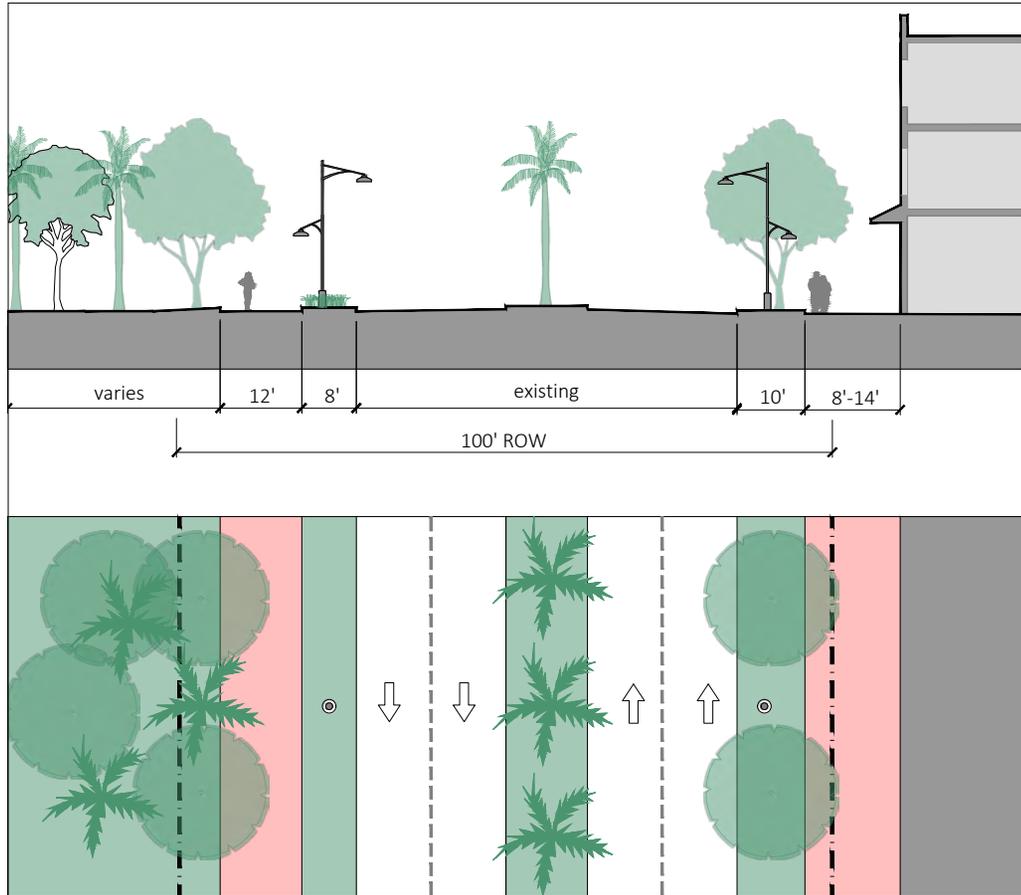
In an effort to promote investment and provide for residential and mixed-use development along the Dixie Highway corridor, this plan recommends creation of the Dixie Highway Corridor Infill Overlay. The following are recommendations for this new zoning opportunity:

- Applies only to the C-1 zoning districts adjacent to Dixie Highway
- Allows for up-to 20 dwelling units per acre residential
- Allows for up-to three stories as measured in Figure 34 on page 113
- Complies with the Downtown Overlay building frontage percentage requirements
- Complies with the streetscape standards beginning Figure 36 on page 116
- Complies with the parking and access requirements of the Downtown Overlay and the parking diagram recommendations provided Figure 37 on page 117
- Complies with the Frontage Type standards beginning Figure 24 on page 91
- Typical and preferred development of this type are the townhouse projects developed fronting US-1 in many coastal cities of Palm Beach County including the City of West Palm Beach and Delray Beach (see examples below).



DIXIE HIGHWAY CORRIDOR IMPROVEMENTS

FIGURE 31 DIXIE HIGHWAY IMPROVED



DIXIE HIGHWAY IMPROVEMENTS

This section of Dixie illustrates the entire width of the street including enhancements to the areas adjacent to the FEC Railway. In addition to the provision of additional shade trees, this proposal focuses on the creation of a new shared-use path and landscaped buffer on both the east and west sides of Dixie Highway. This should be coordinated with any planned FDOT improvements.

The existing sidewalk along the west side of N. Dixie highway adjacent to the FEC Railway is improved to a 12' width with an 8' landscaped buffer. There is currently no sidewalk along the west side of S. Dixie Highway, yet there are transit stops which are challenging to access. The drawing illustrates a continuation of the improved North Dixie highway shared-use path. An easement request from the FEC Railway is needed for the construction of a shared-use path connecting the corridor uniformly and giving access to transit.



## GENERAL CODE MODIFICATIONS

A number of desired and important code modifications will be necessary to make development application reviews and approvals more consistent and predictable within the Town of Lantana. In addition, certain unmanaged practices that have been overlooked over time should be identified and addressed as conditions within the Town have evolved. Below is a brief list of some of the code modifications that should be prioritized:

- Prohibit non-pervious treatment (shellrock, asphalt, pavers, concrete, etc.) of residential swales and limit the non-pervious area of private residential lots. The aggregate affect of additional non-pervious treatments within the neighborhoods is flooding and these conditions will not improve on their own.
- Set strict standards for the treatment of construction sites including the containment of dirt, sediments, and debris; the access and parking of construction vehicles; and general visual appearance. Also, set specific timelines for completion of construction (to be administered by the Development Services Director).
- Include the requirement for on-site bicycle parking with the substantial expansion of existing commercial or mixed-use projects or any new development requiring site plan approval. Areas in downtown should be prioritized for bicycle parking particularly on Town owned properties.
- Updates to the Town landscaping requirements should include "friendly fertilizer" elements that identify eco-sustainable and non-toxic fertilizers, new xeriscape standards, and a requirement for shade trees instead of palm trees for new development.
- There should be greater discretion provided for Development Services Director approval for unique conditions and circumstances particularly where contributing and non-conforming structures and/or uses are concerned.

There many other areas of the code that need to be addressed but as it relates to the objectives of this plan and planning process, these items should be highlighted.



GENERAL CODE MODIFICATIONS

MEASURING BUILDING HEIGHT

One LDR recommendation is that emphasis be placed on measuring the maximum height of a building in number of stories. The diagram below offers recommendations for where building height measurements should be taken (top of roof deck or the roof eave). In addition, the diagram to the lower left shows how more generous modern floor-to-ceiling heights can exceed 30'-0" in a three story building. Regulating building height by the number of stories, rather than the number of feet, results in a built scale that is predictable to both lay-people and potential developers. Limiting buildings solely by their height in feet can inadvertently encourage developers to maximize building height, and then subdivide into as many stories as possible. Conversely, limiting building height by the number of stories results in authentic architectural variation among buildings and higher, more desirable ceiling heights.

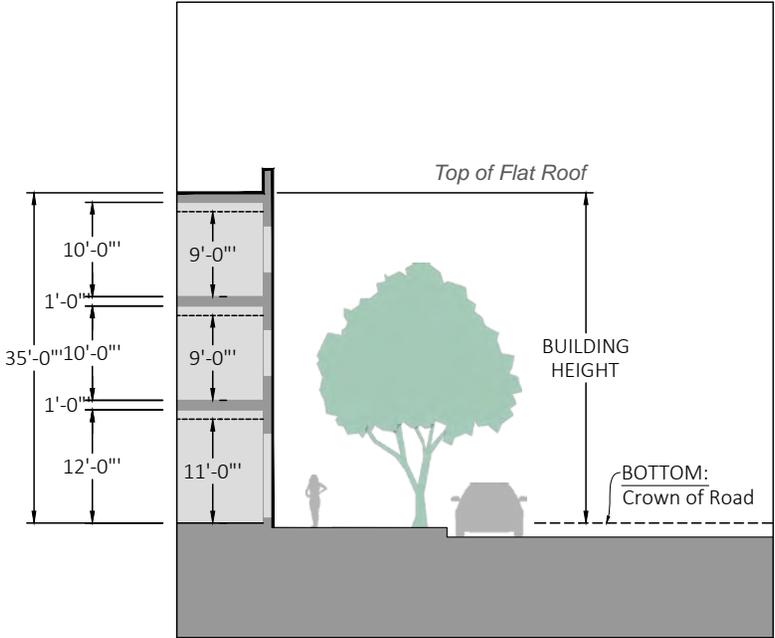


FIGURE 32 BUILDING HEIGHT (TYPE 1)

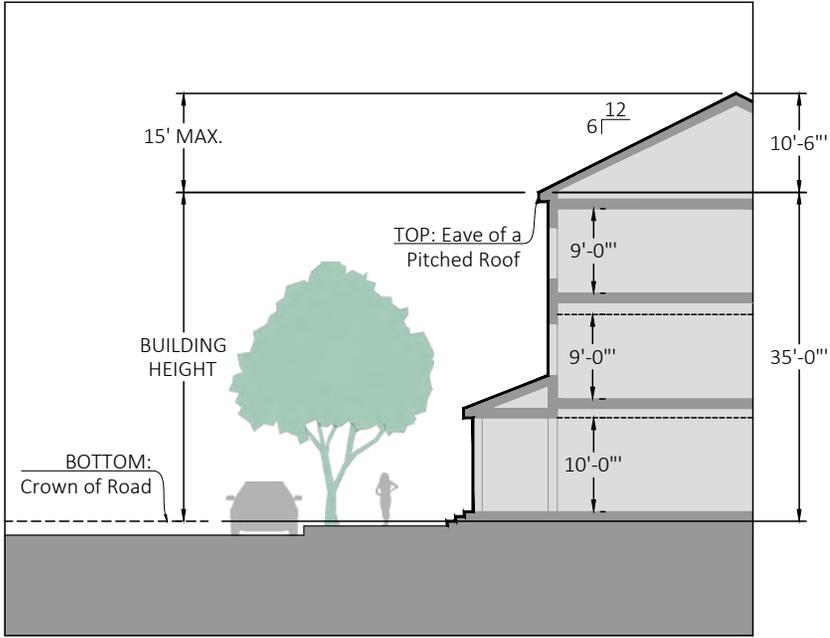


FIGURE 33 BUILDING HEIGHT (TYPE 2)



GENERAL CODE MODIFICATIONS

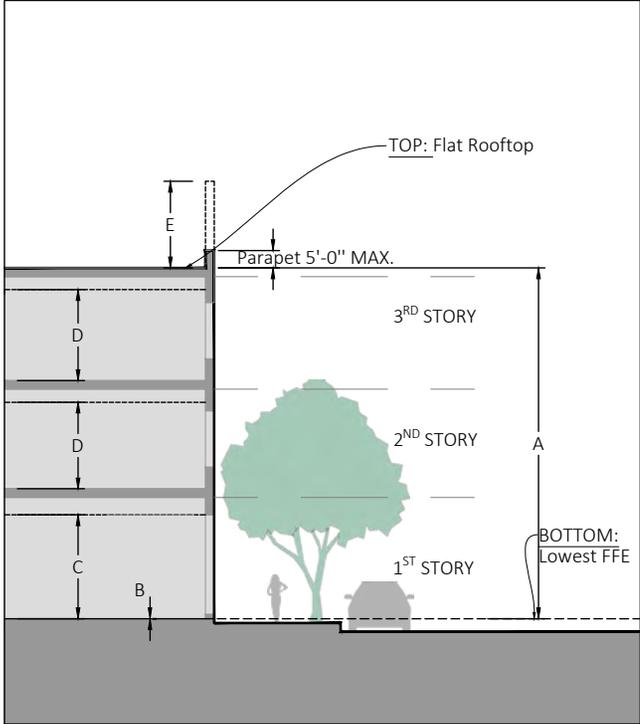
MEASURING BUILDING HEIGHT

The height of buildings shall be measured in and regulated by the number of stories. Stories are measured from the floor to the bottom of the lowest structural member that supports the story above, see Figure 34.

- The ground story of commercial or mixed-use buildings shall be 10 feet to 18 feet tall.
- The ground story of residential buildings shall be from 9 feet to 14 feet tall.
- Each story above the ground story in all buildings must be from 8 feet to 12 feet tall; any upper story taller than 12 feet will count as two stories for the purpose of measuring building height.
- Mezzanines that exceed 15% of the floor area are counted as stories for the purpose of measuring height.

FIGURE 34 BUILDING HEIGHT		
A	Maximum Number of Stories	See note 1
B	Ground Floor Finish Level	18" min.
C	Ground Story Height	10' min. / 18' max.
D	Upper Story Height	8' min. / 12' max.
E	Parapet Height <sup>2</sup>	Existing zoning applies

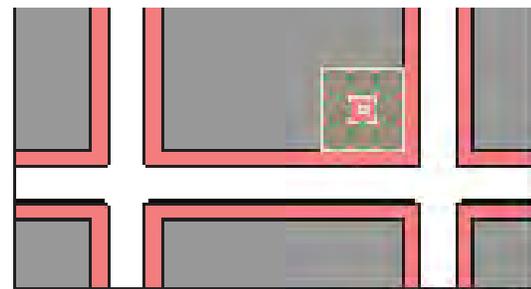
<sup>1</sup> Three stories in Downtown Overlay and US-1 Corridor Infill Overlay. Five stories in Town Center Redevelopment Overlay.  
<sup>2</sup> Structures for the housing of elevators, stairways, skylights, or similar facilities are permitted if necessary to conceal rooftop utilities. May be erected no more than forty (40) percent above the measured building height of the building on which such structures are located.



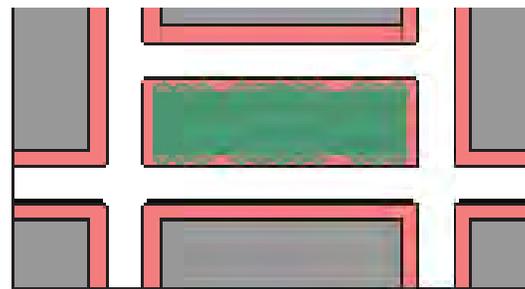
## GENERAL CODE MODIFICATIONS

### CIVIC OPEN SPACE

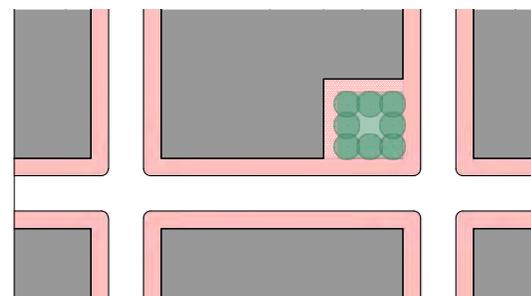
Often open space requirements for developments are satisfied through setbacks, buffers, and residual landscape areas such as parking islands. This plan recommends that the open space requirements be more clearly defined as usable Civic Open Space in the form of parks, greens, and public squares. These alternative civic open space configuration diagrams are a potential tool to more clearly define how future development civic open space requirements should be delivered.



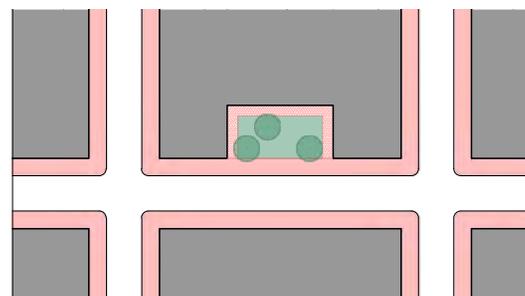
PLAZA



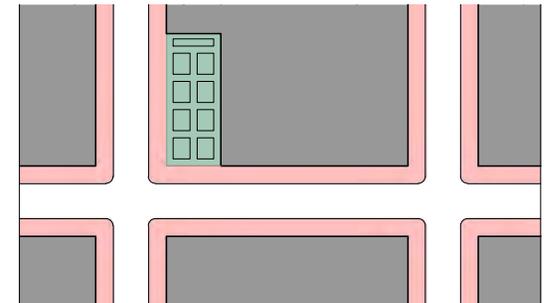
SQUARE



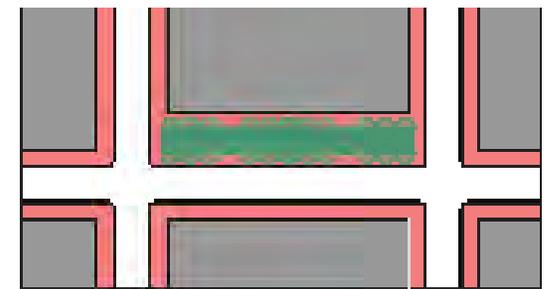
GREEN



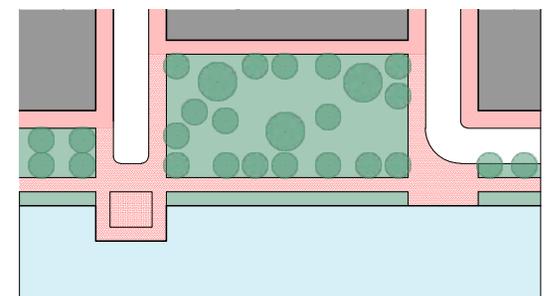
COURTYARD



COMMUNITY PLAYGROUND



ATTACHED GREEN

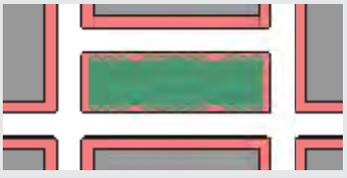
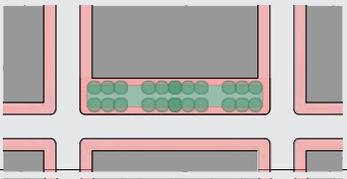
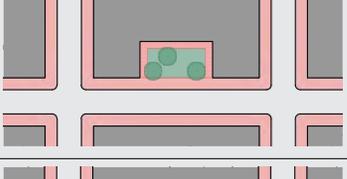
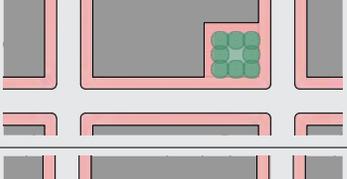
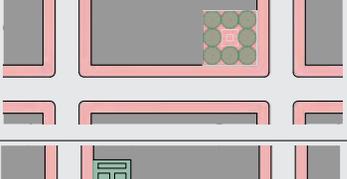
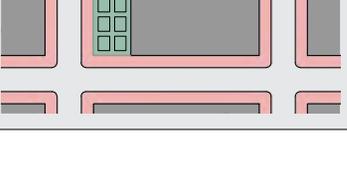


WATERFRONT GREEN

CIVIC OPEN SPACE

The diagrams to the right define a variety of civic open space types and provide dimensional criteria. Every effort should be made, especially where building height incentives are considered to provide civic open spaces of the highest quality.

The parameters described in this table should be considered a practical guide and not limit creativity or application.

	SIZE	DESCRIPTION	ILLUSTRATION
<b>SQUARE</b>	≥ 10,000 sf	A square adjoins streets on at least three sides. Squares may be up to 50% hardscaped, with formal landscaping and shade trees. Squares accommodate both passive uses and community gatherings.	
<b>ATTACHED GREEN</b>	2,000 to 6,000 sf	An attached green spans the entire length of a block. Attached greens shall be at least 30 feet wide and are appropriate on the short end of a block. Attached greens are primarily lawns with formally arranged landscaping and shade trees.	
<b>COURTYARD</b>	Courtyard space ratio of height to width: Min. = 1:1 Max. = 1.5:1	An uncovered area for pedestrians partly or wholly enclosed by buildings or walls and used primarily for supplying access, light, and air to abutting buildings.	
<b>GREEN</b>	1,000 to 5,000 sf	A continuous area for pedestrians which is open from the ground level to the sky for its entire width and length, the primary feature of which is a landscaping scheme that incorporates garden elements including trees, palms, shrubs, or ground cover, as well as water elements including a fountain or pond.	
<b>PLAZA</b>	1,000 to 43,000 sf	Fronts on the street and is directly accessible to the public at all times for use by the public for passive recreational purposes. The ground level of the plaza shall be constructed principally of hard-surfaced materials. An existing unimproved area between or next to a building or buildings shall not qualify. Should not be near another plaza.	
<b>PLAYGROUND</b>	There is no minimum or maximum size	An open space designed and equipped for the recreation of children, and should be fenced and may include an open shelter. Playgrounds may be included in parks and greens.	

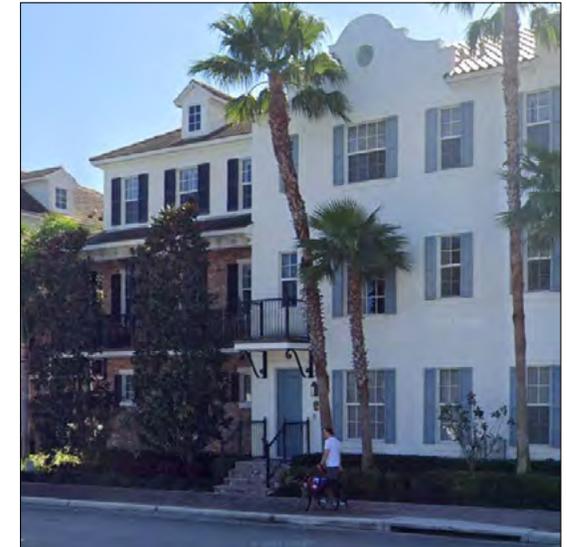
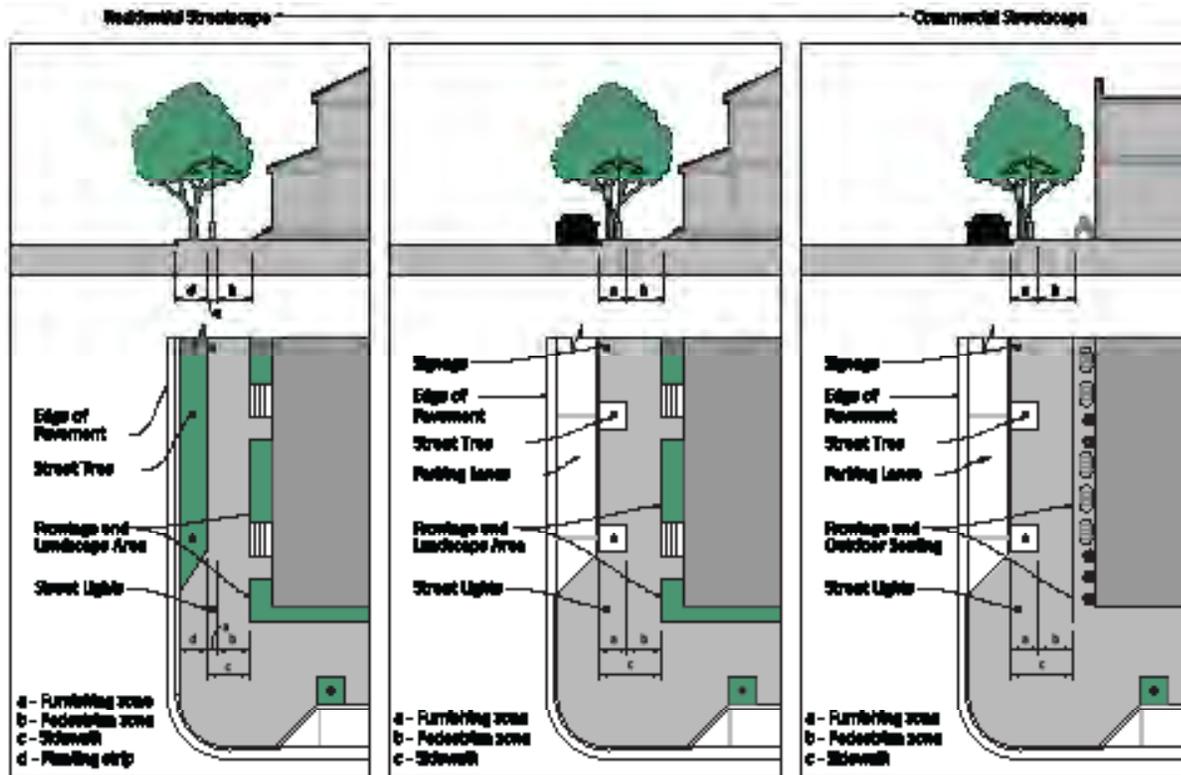
**FIGURE 35** OPEN SPACE CONFIGURATIONS DIAGRAM



GENERAL CODE MODIFICATIONS

STREETSCAPE

The sections below illustrate the subtle changes in streetscapes as they transition from strictly residential conditions to commercial and mixed-use conditions. The specific dimensions for the Furnishing Zone, Pedestrian Zone, outdoor seating, and planting areas may vary based upon existing conditions and the limits of a project’s scope of work. For the purposes of future code updates and discussions with specific development applicants, the Town may want to provide precise dimensions, a range of dimensions, or a minimum dimension for each zone depending upon the conditions on the ground.



Residential streetscape conditions for townhouses in Delray Beach

FIGURE 36 STREETSCAPE TRANSITIONS



GENERAL CODE MODIFICATIONS

CONCEALED AND EXPOSED PARKING

The provision of adequate vehicle parking is an essential component of maintaining healthy and vibrant business environments. The quantity and location of on-site parking requirements can also become a detriment to pedestrian movements and the quality of a place. Conventional standards of front-loaded parking lots, and the requirement of an over-abundance of parking spaces, has resulted in the degradation of the public realm and made the pedestrian and non-motorized environments challenging. Buildings located closer to the street are easier to access by transit users and other pedestrians and bicyclists.

In downtown Lantana the provision of shared parking arrangements between existing businesses, off-site municipal parking, way finding signage, valet service, and potential electric shuttle service are all avenues that should be explored to best maximize existing parking quantities.

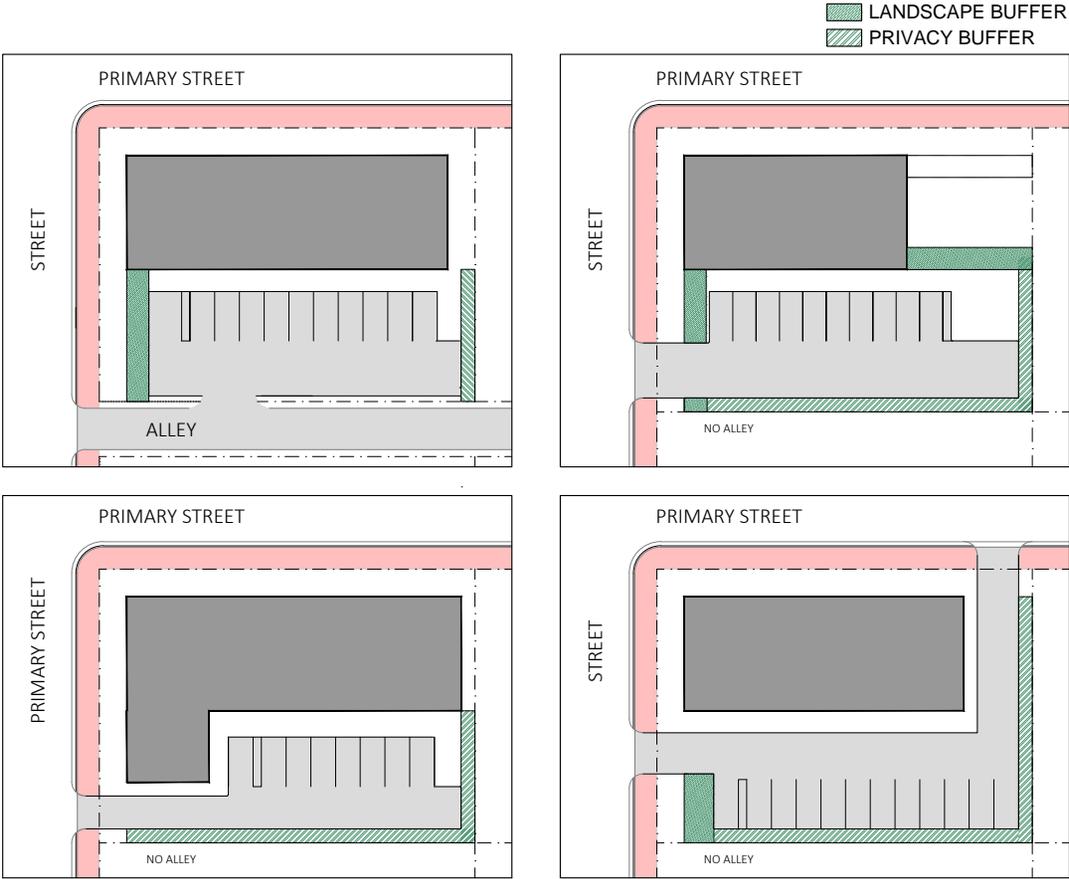


FIGURE 37 PARKING LOCATIONS

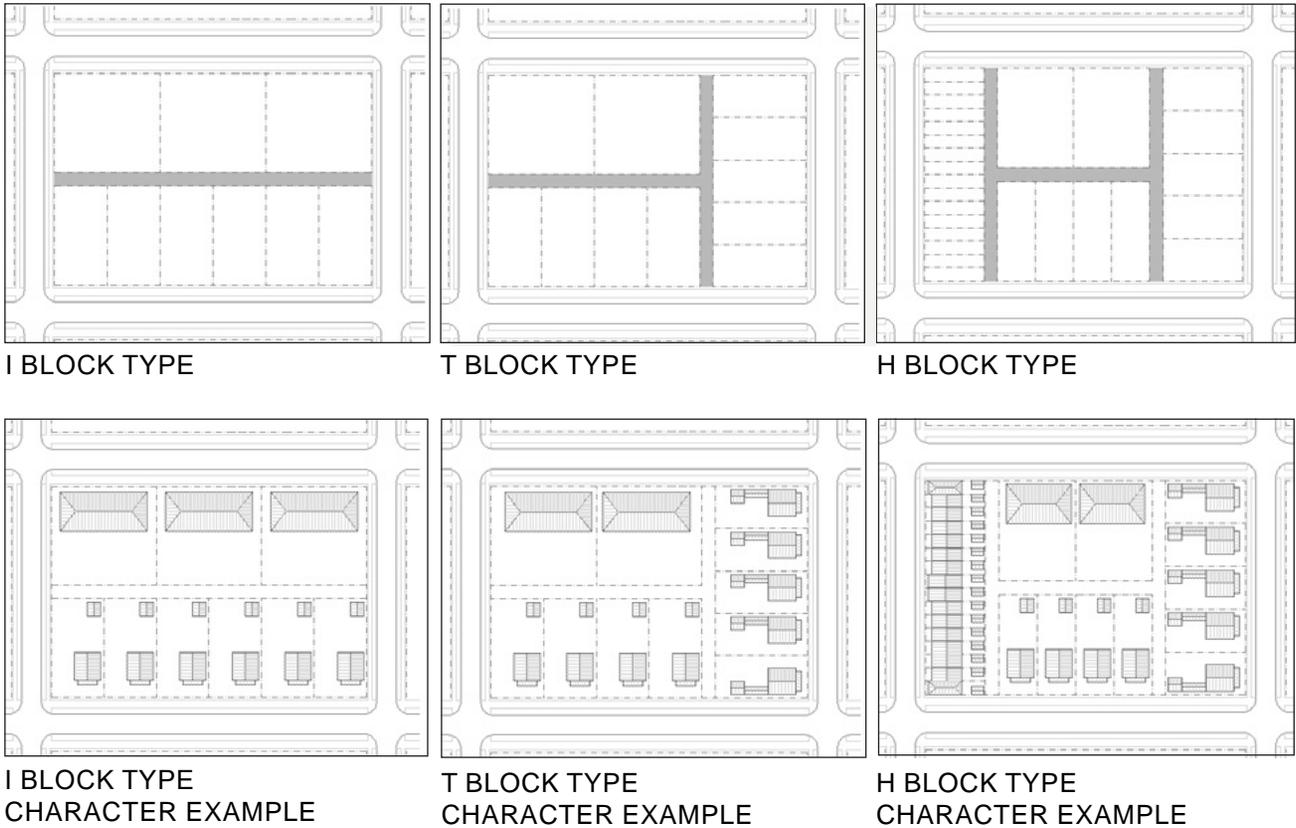


GENERAL CODE MODIFICATIONS

STREET AND BLOCK STANDARDS

As commercial properties in Lantana pursue redevelopment as residential or mixed-use projects, it is vital that they develop new street and block systems. This is especially true if the proposed redevelopment sites are larger in size. It is essential that the internal and external connectivity of new redevelopment projects be held to the highest standard. Creating streets and blocks that are walkable, create public spaces, and are disciplined in how buildings are arranged is paramount to successful redevelopment. The diagram to the right illustrates different block types, ideally with alleyways, and how traditionally they are arranged. Interconnections between adjacent sites and parcels without use of the primary street network are critical for efficiency and safety.

FIGURE 38 BLOCK STANDARDS



These rules should apply to locations such as the Winn Dixie/Kmart site.



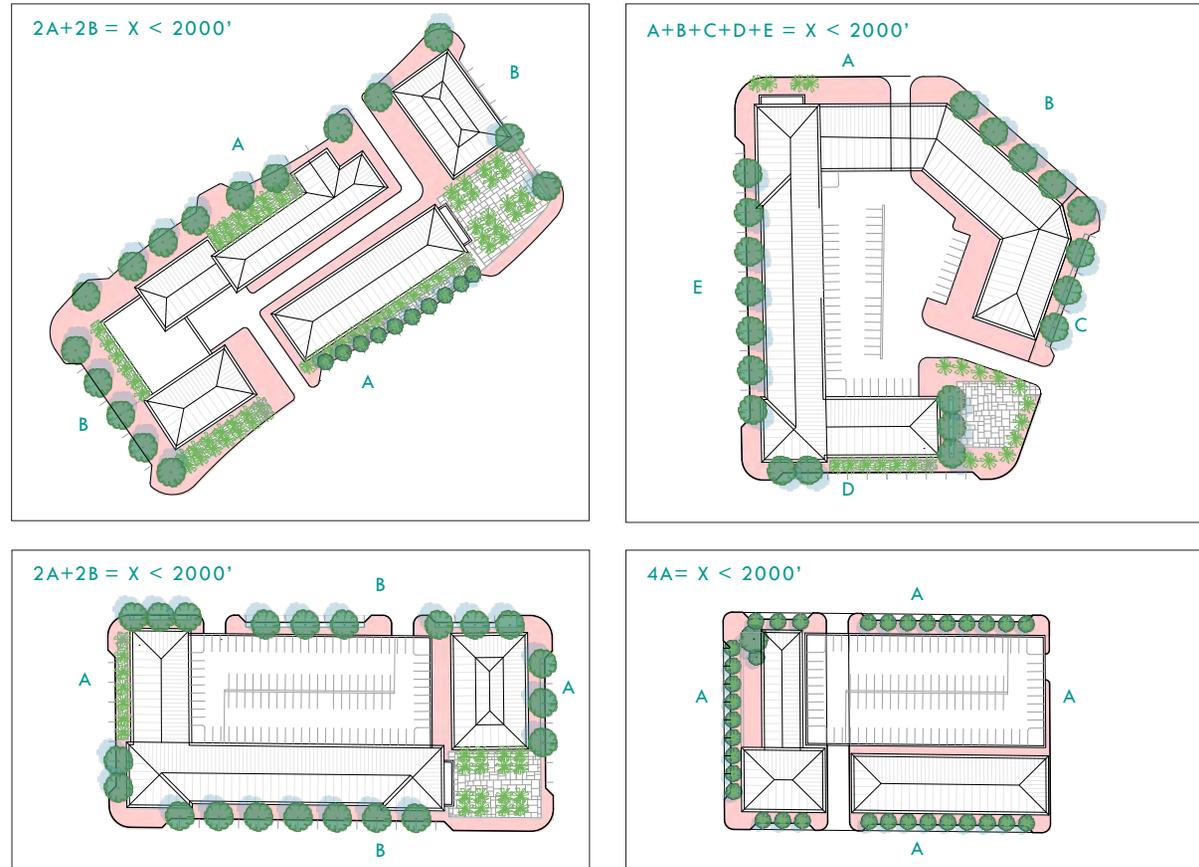
## GENERAL CODE MODIFICATIONS

### BLOCK STANDARDS

The block types and how buildings are arranged on them, as illustrated on the previous page, is very important. So too is the block size. The creation of very large or “super” blocks breaks down the permeability and walkability of a project. Worse yet is the lack of any street and block structure, just the random placement of buildings within parking lots as so many suburban “pod” projects are developed.

The diagram to the right illustrates differing block configurations and how the sum of their fronts are measured. The typical block perimeter dimension in downtown West Palm Beach is approximately 1,500’ to 1,750’. In downtown Delray Beach the typical block perimeter dimension is approximately 1,850’. In no circumstance should new blocks in redevelopment projects exceed 2,000’ in their overall perimeter dimension.

**FIGURE 39** BLOCK PERIMETER DIAGRAM



## GENERAL CODE MODIFICATIONS

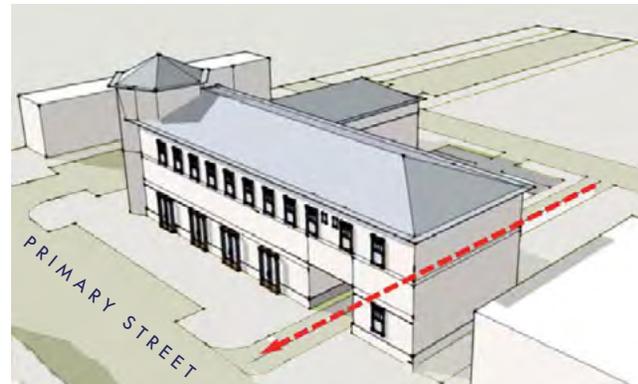
### DRIVE-THROUGH BUILDINGS & GAS STATIONS

The provision of commercial drive-through environments along the commercial corridors in Lantana was an issue brought up during the charrette. There are valid concerns that allowing drive-throughs can erode the public realm and impact the potential for multi-modal environments along the corridors. One approach would be to simply prohibit drive-throughs, however there are other physical design requirements that can accommodate drive-throughs and maintain urban building frontage continuity along the street. The following diagrams illustrate different ways drive-throughs can be accommodated in an urban environment.

Limiting drive-through access from primary streets and only allowing access from side streets or alleyways will lessen the impact to the public realm from the drive-throughs. In addition, fuel pumps at filling stations should be located to the interior of the site and screened from view from a primary street.



“REVERSE” GAS STATION



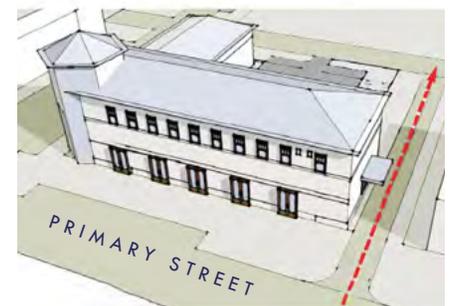
DRIVE-THROUGH CONFIGURATION 1

*Circulation and stacking travel through the building. Cars enter from the rear of the lot and exit to the primary street.*



DRIVE-THROUGH CONFIGURATION 2

*Drive-through stacking occurs in the rear of the lot. Circulation is from the rear of the lot, exiting to a non-primary street.*



DRIVE-THROUGH CONFIGURATION 3

*Circulation and stacking occurs along the side of the building. Cars enter from the rear of the lot, exiting to the primary street.*

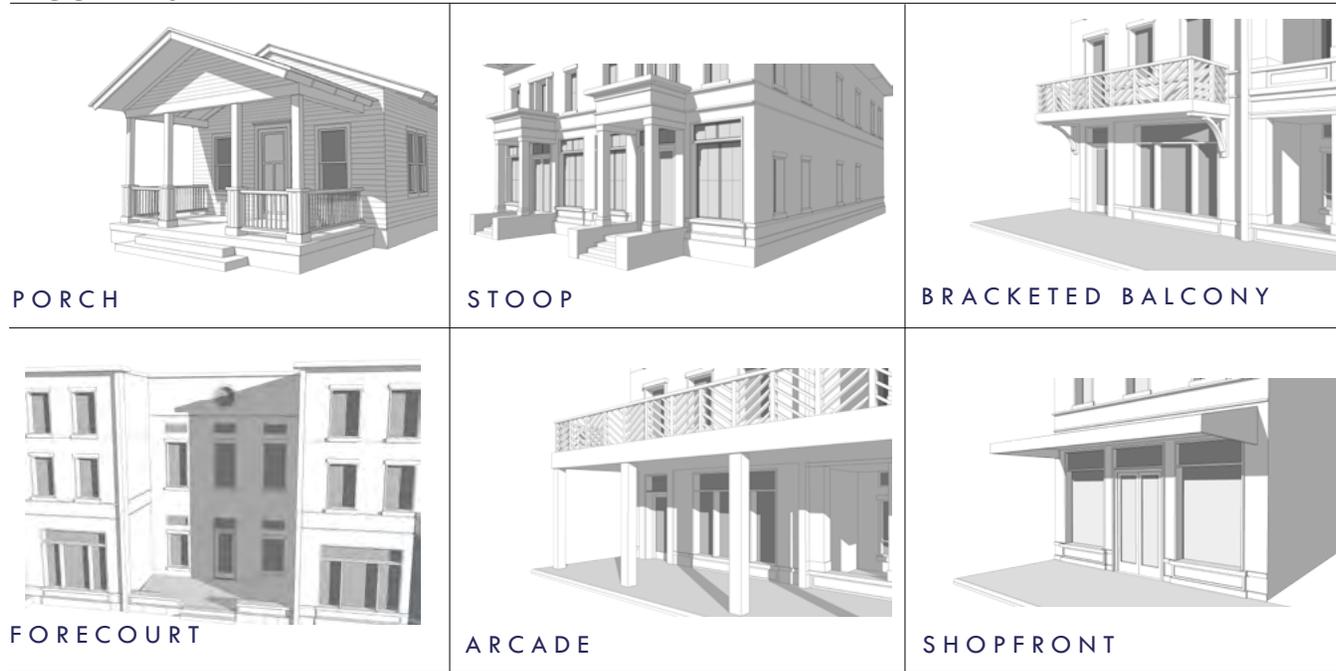


GENERAL CODE MODIFICATIONS

FRONTAGE TYPES

This compendium of building Frontages Types are assigned to Primary Streets identified in the different districts. Not all frontages are appropriate for all streets. As an example, a storefront frontage type would not be appropriate on a strictly residential street just as a porch type would not be appropriate on US-1. By controlling the Frontages Types, along with the suggested street section changes, the public realm of Lantana should be well protected as redevelopment occurs. The following pages recommend Frontage Types per district.

FIGURE 40 FRONTAGE TYPES MATRIX



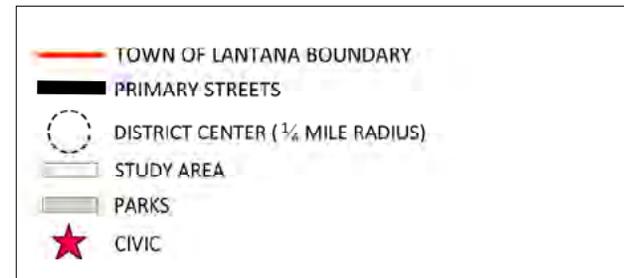
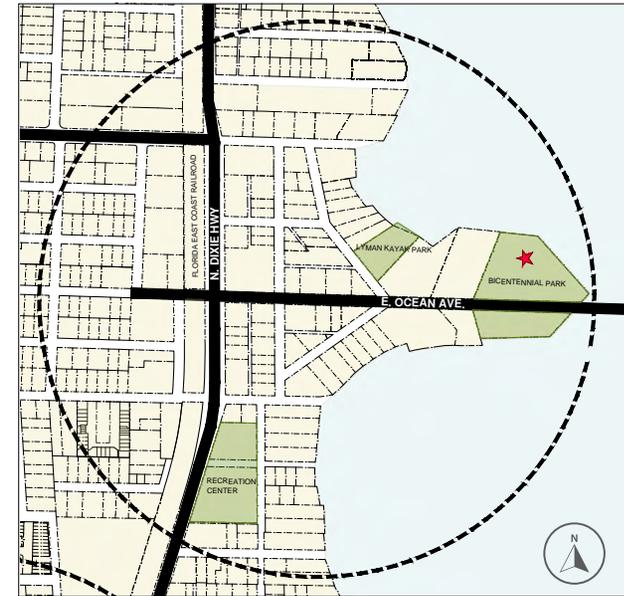
## GENERAL CODE MODIFICATIONS

### DOWNTOWN FRONTAGE TYPES

Frontage Types describe the way that different buildings address the street through their entry features and other defining elements. Not all frontage types are appropriate for all streets. As an example, a storefront frontage type would not be appropriate on a strictly residential street just as a porch type would not be appropriate on US-1. By controlling the Frontages Types, along with the suggested street section changes, the public realm of Lantana should be well protected as redevelopment occurs. This compendium of building Frontage Types are assigned to streets identified for Dixie Highway and E. Ocean Avenue in downtown.

**FIGURE 41** PERMITTED FRONTAGE TYPES MATRIX

DOWNTOWN		
PERMITTED FRONTAGE TYPES	DIXIE HWY.	E. OCEAN AVE.
Porch	✗	✓
Stoop	✓	✓
Bracketed Balcony	✓	✓
Forecourt	✓	✓
Arcade	✓	✓
Shopfront	✓	✓



## CODE RECOMMENDATIONS

### TOWN CENTER FRONTAGE TYPES

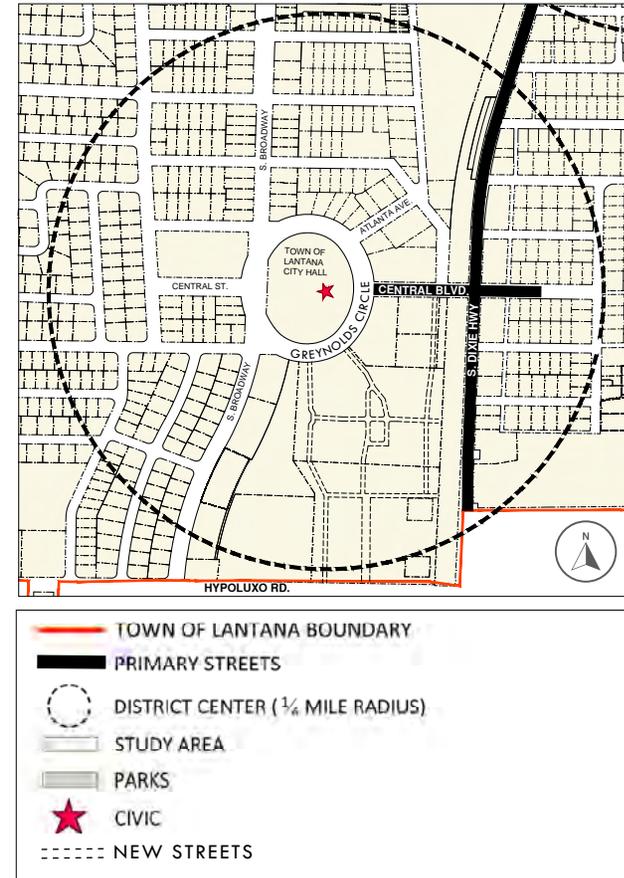
#### Frontage Types

The Frontage Types appropriate for different corridors in the Town Center District are identified below. Note that a New Street column has been added to apply to the Kmart redevelopment area.

The assignment of appropriate Frontage Types in the table below have to do with continuing and improving the existing character and context of the particular street. While a typical single family residential porch is not appropriate on S. Dixie Highway, a more urban Stoop Frontage Type as found in a townhouse project is. These Frontage Type matrices are intended to give visual guidance to applicants and staff and may be recommendations or embedded in the code as regulations.

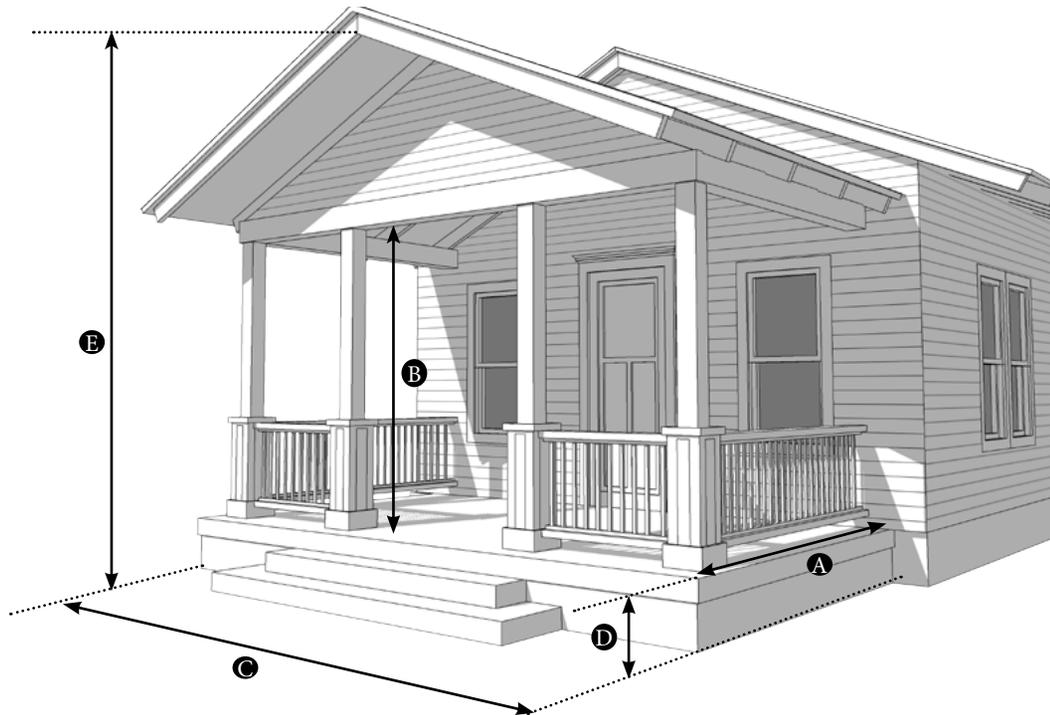
**FIGURE 42** PERMITTED FRONTAGE TYPES MATRIX

TOWN CENTER			
PERMITTED FRONTAGE TYPES	S. DIXIE HWY.	CENTRAL BLVD.	NEW STREETS (K MART SITE)
Porch	✗	✓	✓
Stoop	✓	✓	✓
Bracketed Balcony	✓	✓	✓
Forecourt	✓	✓	✓
Arcade	✓	✓	✓
Shopfront	✓	✓	✓



## FRONTAGE TYPES

**FIGURE 43** PORCH FRONTAGE



### DESCRIPTION

A porch is an open-air structure attached to a building forming a covered entrance large enough for comfortable use as an outdoor room. Front porches may be screened.

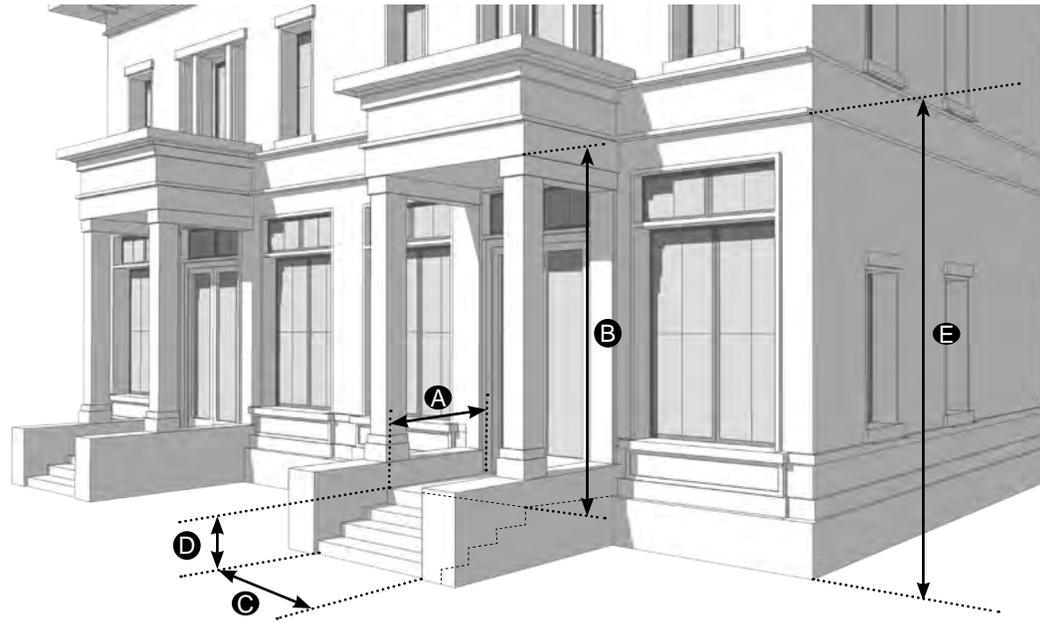
### DIMENSIONS

Depth	6 feet min. 8 feet preferred	A
Height, clear	8 feet min.	B
Width, length of facade	40% min.	C
Finish level above finished grade	21 inches min.	D
Height, stories	2 stories max.	E
Set back from curb	Not applicable	-



## FRONTAGE TYPES

**FIGURE 44** STOOP FRONTAGE



### DESCRIPTION

A stoop is a small staircase leading to the entrance of a building that may be covered. The elevation of the stoop is necessary to ensure privacy for residential uses in the ground story of buildings. Stoops should provide sufficient space for a person to comfortably pause before entering or after exiting the building.

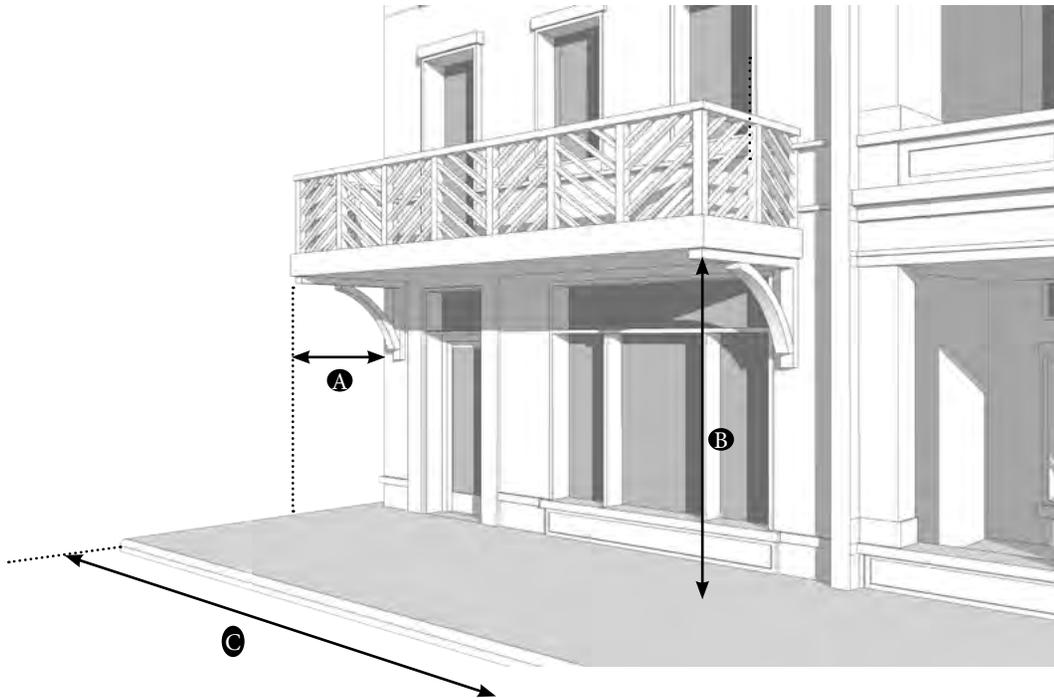
### DIMENSIONS

Depth	5 feet min.	<b>A</b>
Height, clear	8 feet min.	<b>B</b>
Width, clear	4 feet min.	<b>C</b>
Finish level above finished grade	21 inches min.	<b>D</b>
Height, stories	1 story max.	<b>E</b>
Set back from curb	Not applicable	-



## FRONTAGE TYPES

**FIGURE 45** BRACKETED BALCONY FRONTAGE



### DESCRIPTION

A bracketed balcony is a second-story balcony, that creates a semi-public space overlooking the street above a main entry or unit. Bracketed balconies are typically associated with buildings with commercial uses in the ground story; however, bracketed balconies may be used with residential uses and in combination with a storefront or a stoop.

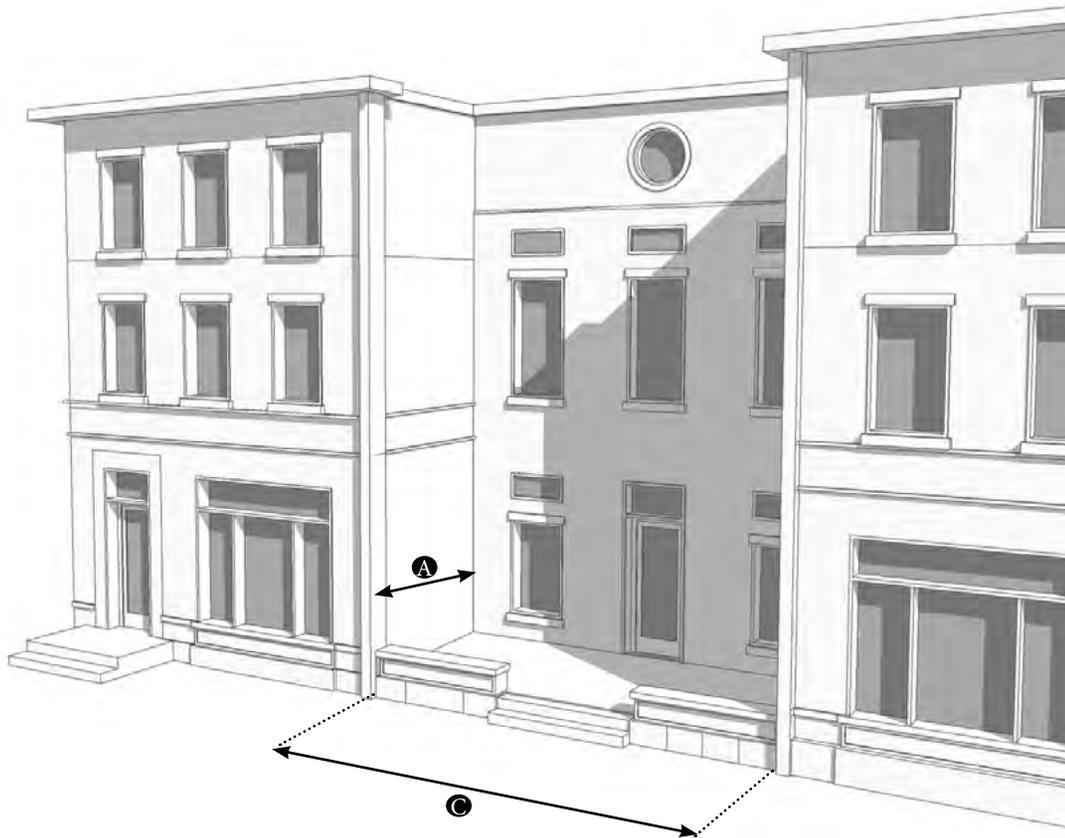
### DIMENSIONS

Depth	5 feet max.	<b>A</b>
Height, ground level clear	10 feet min.	<b>B</b>
Width	4 feet min.	<b>C</b>



## FRONTAGE TYPES

**FIGURE 46** FORECOURT FRONTAGE



### DESCRIPTION

A forecourt is an open area in front of the main building entrance(s) designed as a small garden or plaza. Low walls or balustrades no higher than three feet six inches in height when solid may enclose the forecourt. Forecourt walls are constructed of similar material as the principal building or are composed of a continuous, maintained hedge. A forecourt may afford access to one or more first floor residential dwelling units or incorporate storefronts for commercial uses. Forecourts are typically associated with multifamily, mixed-use, and commercial buildings.

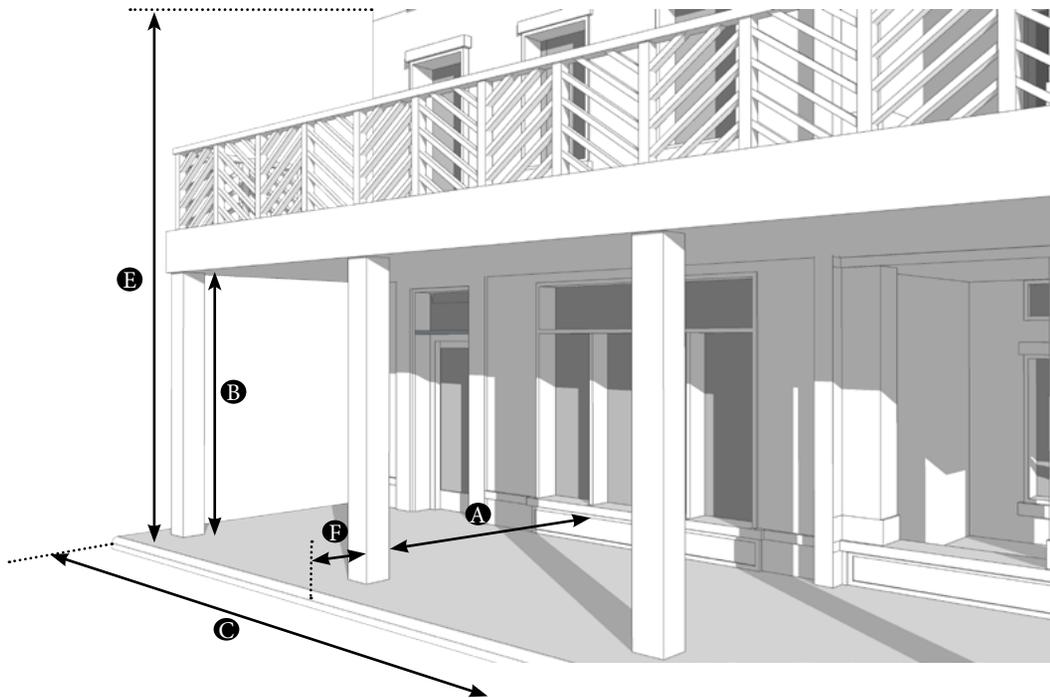
### DIMENSIONS

Depth, clear	20 feet max.	A
Height, clear	Not required	-
Width, length of facade	12 feet min. / 50% of facade max.	C
Finish level above finished grade	Not required	-



## FRONTAGE TYPES

**FIGURE 47** ARCADE FRONTAGE



### DESCRIPTION

An arcade is a covered, unglazed, linear hallway attached to the front of a building, supported by columns or pillars. The arcade extends into the public right-of-way, over the streetscape area, creating a shaded environment ideal for pedestrians. This frontage type is typically associated with commercial uses. Arcades shall remain open to the public at all times. In the case where an arcade encroaches into the public right-of-way, a right-of-way maintenance agreement may be required.

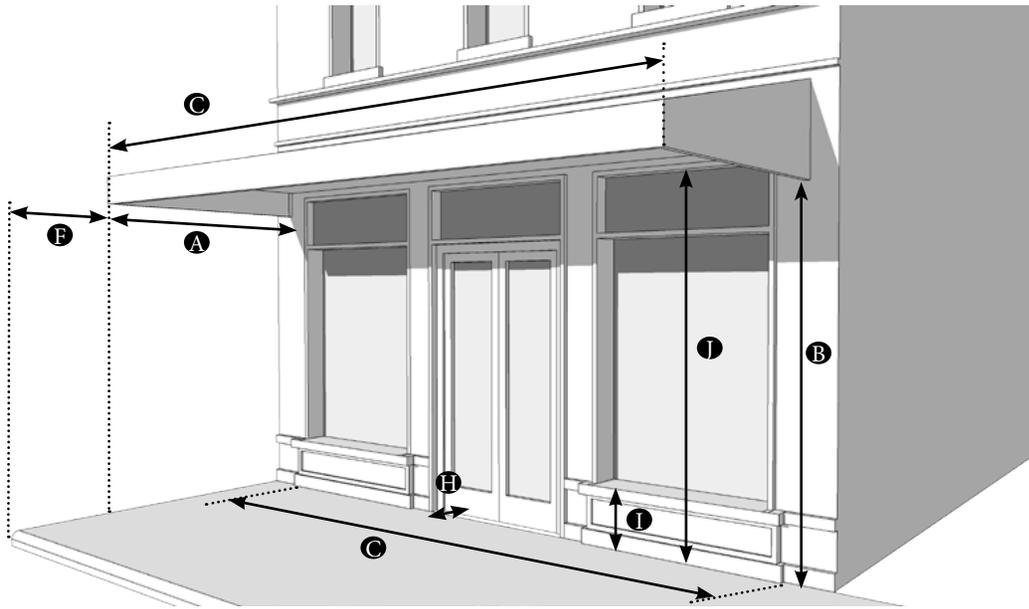
### DIMENSIONS

Depth, clear	8 feet min.	<b>A</b>
Height, ground level clear	10 feet min.	<b>B</b>
Width, length of facade	70% min.	<b>C</b>
Finish level above finished grade	at sidewalk level	-
Height, stories	2 stories max.	<b>E</b>
Set back from curb	2 feet min. / 4 feet max.	<b>F</b>



## FRONTAGE TYPES

**FIGURE 48** SHOPFRONT FRONTAGE



### DESCRIPTION

The shopfront is a frontage type along the sidewalk level of the ground story, typically associated with commercial uses. Shopfront are frequently shaded by awnings or arcades.

### DIMENSIONS

Width, length of facade	70% min.	G
Door recess	10 feet max.	H
Storefront base	1 foot min. / 3 feet max.	I
Glazing height	8 feet min.	J

### OPTIONAL AWNING

Depth	3 feet min.	A
Height, ground level clear	8 feet min.	B
Width, length of facade	70% min.	C
Set back from curb	2 feet min.	F



ADDITIONAL RECOMMENDATIONS

TREE PLANNING AND PLANTING CAMPAIGNS

CONSIDERATIONS TO MAKE BY STAKEHOLDER TYPE

CITY COMMISSIONER/COUNCILOR

Trees increase property values and revenues in shopping districts, improve community health, make the community more resilient to climate change, and make it more attractive to new businesses and entrepreneurs. Trees will pay their way through more economic activity and better tax revenues from increased property values.

CITY PUBLIC WORKS/ENGINEERING

Trees have been shown to take up stormwater, reduce standing water, lowersurface temperatures, extendpavement lifeandimproveairquality in terms of reduced particulates, greater ozone and fewer volatile organic compounds, while sequestering carbon to mitigate climate change. And, if yours is a coastal community, trees also provide a buffer against storms, reduce storm damage to infrastructure, minimize coastal erosion, and so on. Trees clean both the air and water and reduce flooding at a cost far cheaper than engineered solutions, such as stormwater ponds.

The infographic is a collage of images and text. At the top left, a photo shows two people planting a tree. Below it, a photo shows a person pointing at a map. To the right of these is a large white box with the title 'Tree Planning and Planting CAMPAIGNS' in green and blue, and the subtitle 'A Guide for Reforesting Cities and Towns' in blue. Below the title are two photos of people planting trees in a park. To the left of these photos is a vertical red bar with the text 'TREE MAPPING' in white. Below the photos is a green bar with the text 'CANOPY GOALS' in white. At the bottom left, a photo shows a person with a red cart. To its right is a photo of people walking on a path. Below these is a blue bar with the text 'SOCIAL TREE EQUITY' in white. To the right of this bar is a vertical brown bar with the text 'TREE BENEFITS' in white. At the bottom right, a photo shows people on a bridge over water. At the very bottom, there is a logo for 'The Green Infrastructure Center Inc.' with a green grid icon, the text 'The Green Infrastructure Center Inc. June 2022', and the 'UAS' logo with the text 'Funded by the Southern Region of the USDA Forest Service.'



#### TREE PLANNING AND PLANTING CAMPAIGNS

##### CITY HEALTH OFFICIALS/HOSPITALS

Well-treed communities have better respiratory health and fewer hospital visits from chronic conditions, such as asthma. Trees encourage people to walk and bicycle more and farther, thus encouraging heart and lung health and reduced onset of Type II diabetes. Furthermore, patients heal up to 30% faster when they can see or access green spaces, children who suffer from Attention Deficit Hyperactivity Disorder (ADHD) benefit from living near forests and other natural areas, and children who live closer to green areas have improved cognitive function.

##### CITY PARKS AND RECREATION

Parks with good tree canopy are more inviting and healthier for users, for all the reasons outlined above. Trees also provide shade, and thus more options for diverse uses in parks, such as picnics, studying, hiking or outdoor education. Larger natural parks increase the value of nearby parcels more than skate parks or other developed parks, such as golf courses. They also provide greater biodiversity and the opportunity to increase the variety of trees in the city.

##### LOCAL BUSINESS OWNERS

Skilled professionals (also called the “creative class”) seek out communities that are greener and have protections in place for

their parks, street trees and open spaces. So, a green community helps recruit skilled, higher-paid workers with more money to spend in the local community. In general, businesses and cities that are perceived as being green gain a competitive advantage.

##### RESIDENTS/NEIGHBORHOOD ASSOCIATIONS/HOAS

Less crime occurs in well-treed neighborhoods and trees have been shown to improve metabolic rates and moods. Since they increase walkability, more people strolling, jogging, or generally out and about around a neighborhood equates with safer communities and people who interact more as a community and watch out for each other. Trees do not hide criminals or make it easier to rob a home and well treed areas have lower crime rates. What’s more, property values are, on average, 18% higher for well-treed developments. They also save the average household about 20% on its summer energy bills.

##### CONSERVATION/NATURE/GARDEN CLUBS

Native trees support beneficial insects and pollinators, which we need for a healthy food supply. Although trees in forests are usually pollinated by the wind, understory plants and some broadleaf forest trees rely on pollination services and so provide forage for native pollinators. A mature oak tree supports up to 534 species of insects, including moths and butterflies - key pollinators - which is more than any other native tree species. Trees support a number of mammals, which also aids biodiversity and a healthy environment, especially if there is a



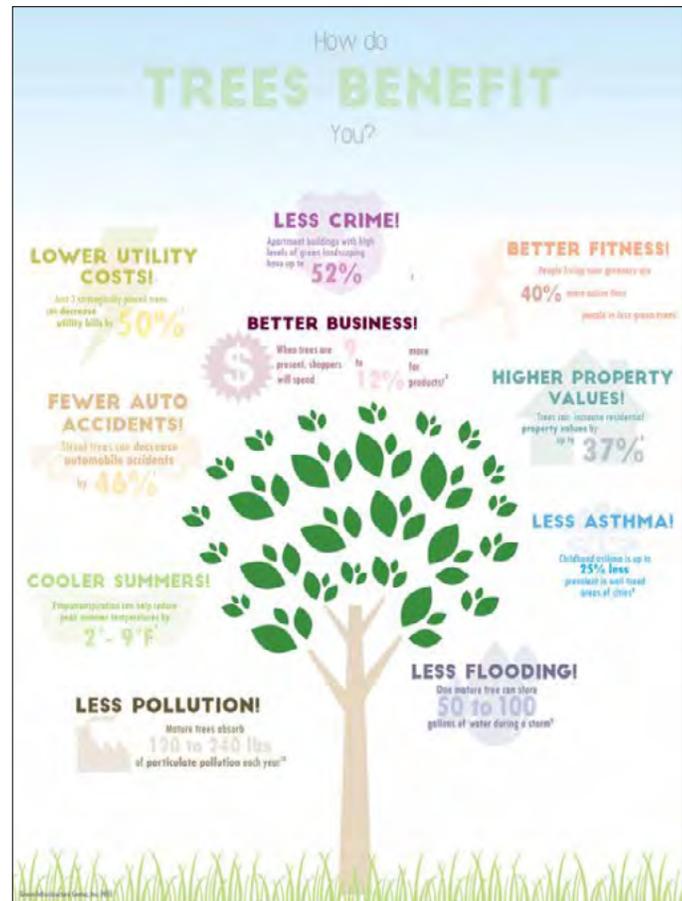
#### TREE PLANNING AND PLANTING CAMPAIGNS

large, wooded area adjacent to the development. They also clean the air and water of pollution and support healthy soil formation. In addition, tree shade reduces heat stress on both animals and people.

#### LARGE LANDHOLDERS

If you are the owner of a significant forested landscape, it's important to know its extent and its health. A privately owned forest provides many public benefits and may serve as a critical connector to other, off-site woodland areas. Forests provide habitat to pollinators that support our food supply, they sequester and clean greenhouse gases such as carbon dioxide and ozone, filter air pollutants and support native wildlife and songbirds, and capture stormwater pollutants from water and recharge aquifers. They also provide buffers against noise and road impacts. Surveys for pests, invasive species or other diseases can help flag problems that need to be addressed early to ensure a healthy forest long into the future.

These excerpts on tree planning and planting are sourced from: "The Tree Planning and Planting Campaigns - A Guide for Reforesting Cities and Towns" The complete guide can be found using this link: [TreePlantingCampaignGuide\\_GIC\\_June2022.pdf](#).

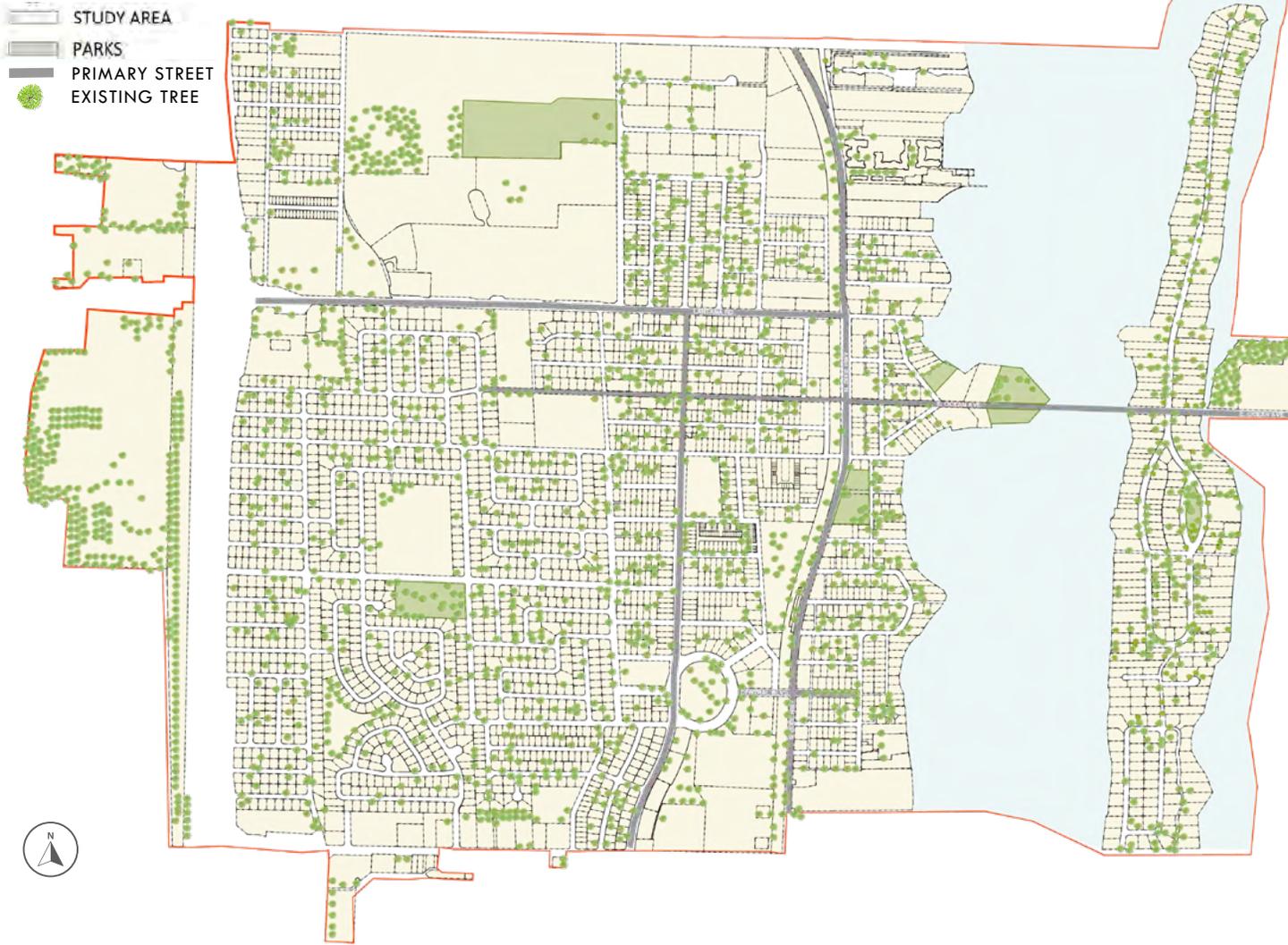


The infographic above is a poster for community events and presentations. For additional statistics on the many benefits trees provide, use the link below for the full Green Infrastructure Guide and see the Appendix for citation references. Contact GIC to obtain a poster-sized copy of this graphic for display. [TreePlantingCampaignGuide\\_GIC\\_June2022.pdf](#)



# INCREASE THE TOWN'S TREE INVENTORY

**FIGURE 49** LANTANA TREE CANOPY DIAGRAM



## INTRODUCTION

In 2013 the Town of Lantana was recognized by the Florida Urban Forestry Council and was awarded the statewide Outstanding Public Education Award for its Arbor Day Program. Unfortunately the Town's tree canopy has diminished over time and many of the primary and neighborhood streets are exhibiting a deficit of shade trees.

The map to the left illustrates the existing tree canopy in town today based upon recent aerials. The following pages identify areas where focused shade tree planting should occur.



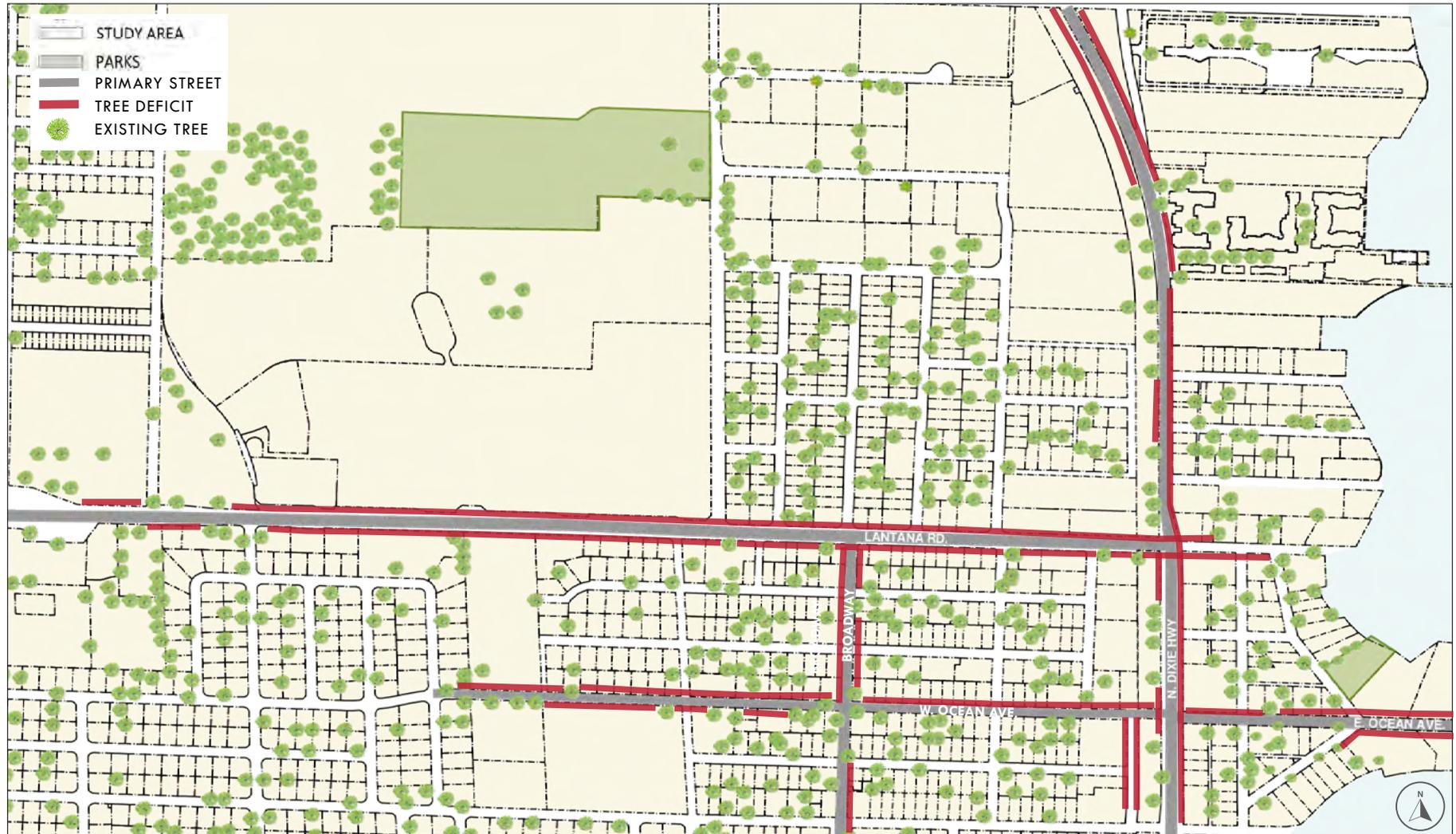
# INCREASE THE TOWN'S TREE INVENTORY

**FIGURE 50** TREE CANOPY DEFICIT DIAGRAM



# INCREASE THE TOWN'S TREE INVENTORY

**FIGURE 51** TREE CANOPY DEFICIT DIAGRAM



## INCREASE THE TOWN'S TREE INVENTORY CANOPY TREES AND VEGETATION

#### REDUCED ENERGY USE:

Trees and vegetation that directly shade buildings decrease demand for air conditioning.

#### IMPROVED AIR QUALITY AND LOWER GREENHOUSE GAS EMISSIONS:

By reducing energy demand, trees and vegetation decrease the production of associated air pollution and greenhouse gas emissions. They also remove air pollutants and store and sequester carbon dioxide.

#### ENHANCED STORMWATER MANAGEMENT AND WATER QUALITY:

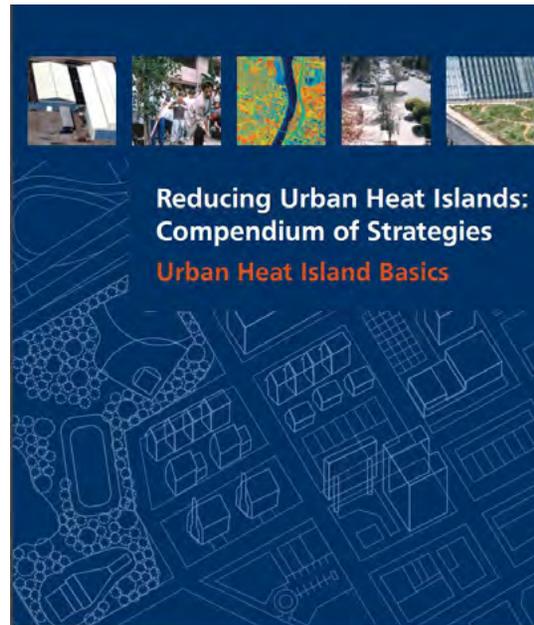
Vegetation reduces runoff and improves water quality by absorbing and filtering rainwater.

#### REDUCED PAVEMENT MAINTENANCE:

Tree shade can slow deterioration of street pavement, decreasing the amount of maintenance needed.

#### IMPROVED QUALITY OF LIFE:

Trees and vegetation provide aesthetic value, habitat for many species, and can reduce noise.



### PRIORITIZE THE TREE CANOPY

- REDUCED ENERGY USE
- IMPROVED AIR QUALITY AND LOWER GREENHOUSE GAS EMISSIONS
- ENHANCED STORMWATER MANAGEMENT AND WATER QUALITY
- REDUCED PAVEMENT MAINTENANCE
- IMPROVED QUALITY OF LIFE

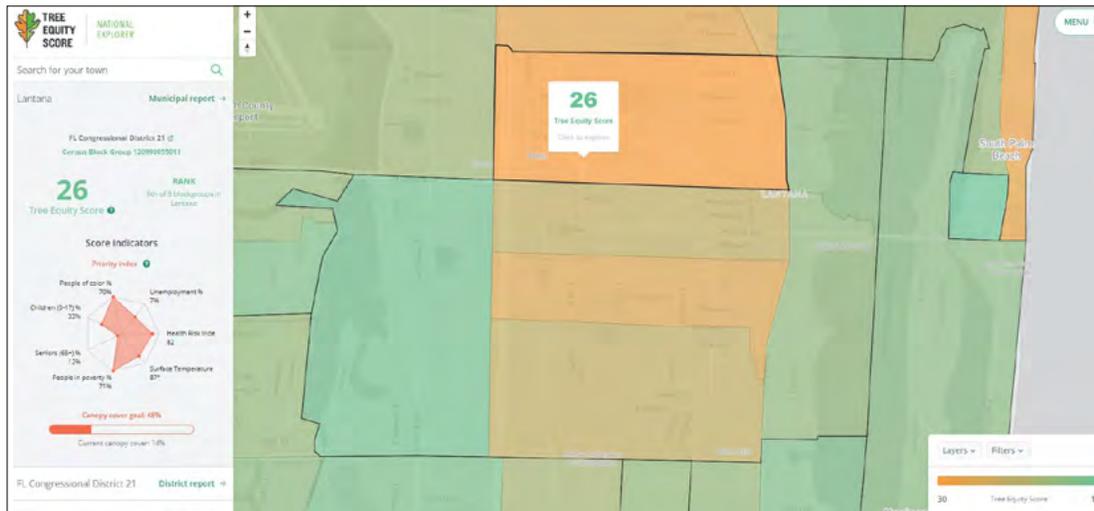
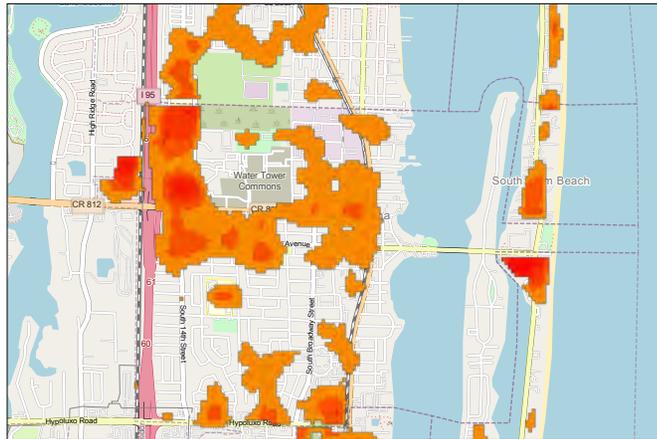
U.S. Environmental Protection Agency. 2008. Reducing urban heat islands: Compendium of strategies.

[Urban Heat Island Compendium](https://www.epa.gov)

<https://www.epa.gov>



## REDUCING THE TOWN'S HEAT ISLAND EFFECT CANOPY TREES AND VEGETATION



### PRIORITIZE THE TREE CANOPY

Of the many benefits of have a robust shade tree canopy in the community, the reduction of the heat island effect is one that is well documented.

Reducing a community's heat retention through the provision of shade trees not only helps the environment but makes outdoor spaces such as parks and sidewalks more useful and can have an overall positive effect on property values.

The information at left was acquired from the Tree Equity Score website which maps the relationships between tree canopies and property values.



### III. KEY RECOMMENDATIONS

#### **LIVE OAK** *QUERCUS VIRGINIANA*

##### ATTRIBUTES

- Grows rapidly when young. Can live for centuries. Adapts to almost any type of soil. Tolerates salt spray and compacted soil.
- Works well as a street tree. Has superior wind resistance.
- Assumes a dwarf form on drier sites.
- Grows in a rounded shape. A live oak tree is a moderate grower, it needs full sun and room to spread its wings, as well as a well-drained planting location. Oaks are cold hardy, and fine in any Florida planting zone.
- Trimming is unnecessary for a young live oak tree but watering is critical. These trees must have regular irrigation to grow strong root systems that will support this large a tree.
- Clear zone from sidewalks of 10 feet or more so roots don't cause problems.
- Avoid placing near other big trees that will shade it.

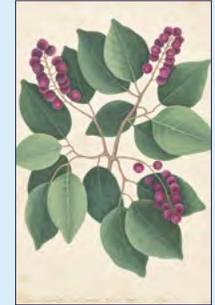


#### **PIGEON PLUM** *COCCOLOBA DIVERSIFOLIA*

##### ATTRIBUTES

It is a Florida native with a nice shape and attractive exfoliating bark. Pigeon Plum also appears on most city-approved tree lists. The Pigeon Plum is very similar in appearance to its Sea Plum and Sea Grape relatives.

- Does well in full sun or part shade. Moderately fast growing in part shade, but in full sun, this plant is thought to be fast growing.
- It throws purple fruit in which birds enjoy eating. Its sweet fruit attracts birds, such as the mockingbird, robin, pigeon, dove, and woodpecker, creating a nice natural habitat for indigenous wildlife. It only drops for 2 months out of the year.
- No need for irrigation after becoming established. It gets a dense and compact canopy.
- This tree is both drought and salt tolerant.
- High wind tolerance makes this a hurricane resistant tree.



<b>LIVE OAK QUERCUS VIRGINIANA</b>		
	Native or Florida Friendly	Yes
	Zone	Zone 7-10
	Height and Canopy	Height of 40–80' and a spread of 60–100' at maturity
	Growth Rate	Medium rate, with height increases of 13–24" per year
	Drought Tolerant	Has some flood and drought tolerance
	Soil preference and Maintenance	Very adaptable—growing in most soils
	Flowering/Fruiting	Produces green acorns
	Fruit/Leaf Litter	Some leaf litter with new growth
	Wildlife Value	Acorns provide food for many birds and mammals



<b>PIGEON PLUM COCCOLOBA DIVERSIFOLIA</b>		
	Native or Florida Friendly	Yes
	Zone	Zone 10b-11
	Height and Canopy	Height of 15-25' and a spread of 20-35' at maturity
	Growth Rate	Medium rate, with height increases of 12-36" per year
	Drought Tolerant	High drought tolerance
	Soil preference and Maintenance	Moist well-drained soils
	Flowering/Fruiting	Producing year-round flowers/fruiting only once a year
	Fruit/Leaf Litter	Dropping fruit only two months out of the year
	Wildlife Value	Sweet fruit attractant to birds



### III. KEY RECOMMENDATIONS



**GREEN BUTTONWOOD**  
*CONOCARPUS ERECTUS*

**ATTRIBUTES**

A Florida native, buttonwood is ideal for seaside plantings. It withstands the rigors of urban conditions very well and makes a durable street or parking lot tree.

- Due to its small size, plant on 15-foot centers to form a closed canopy along a street.
- Purchase single-trunked trees for street and parking lot plantings.
- It is highly tolerant of full sun, sandy soils, and salty conditions.
- It also tolerates brackish areas and alkaline soils, thriving in the broken shade and wet soils of hammocks.



**GUMBO LIMBO**  
*BURSELA SIMARUBA*

**ATTRIBUTES**

Gumbo limbo is ideal for a freestanding specimen on a large property or as a street tree. Lower branches will grow close to the ground, so street trees will have to be trained early for proper development. Locate the lowest permanent branch about 15 feet off the ground to provide enough clearance for a street tree planting.

- Moderately fast growing, with great resistance to strong winds, drought, and neglect. Drought avoidance is accomplished by leaf drop, and growth is often best in drier locations not receiving irrigation.
- The inconspicuous flowers are followed by red, three-sided berries that split into three sections at maturity to reveal a 1/4-inch triangular red seed. The fruit takes a year to ripen and matures in early summer.
- Specimen trees are often grown with branches beginning much closer to the ground, providing a beautiful specimen plant with wonderful bark.



**GREEN BUTTONWOOD CONOCARPUS ERECTUS**

	Native or Florida Friendly	Yes
	Zone	Zone 10b-11
	Height and Canopy	Height of 30-40' and a spread of 20-30' at maturity
	Growth Rate	Moderate growth rate
	Drought Tolerant	High drought tolerance
	Soil preference and Maintenance	Very adaptable—growing in most well-drained soils
	Flowering/Fruiting	Producing non showy flowers/fruiting year round
	Fruit/Leaf Litter	No
	Wildlife Value	Host plant and nectar source for rare butterflies and moths



**GUMBO LIMBO BURSELA SIMARUBA**

	Native or Florida Friendly	Yes
	Zone	Zone 10b-11
	Height and Canopy	Height of 20-60' and a spread of 20-50' at maturity
	Growth Rate	Rapid growth rate
	Drought Tolerant	High drought tolerance
	Soil preference and Maintenance	Very adaptable—growing in most well-drained soils
	Flowering/Fruiting	Producing flowers/fruiting in the Spring
	Fruit/Leaf Litter	No
	Wildlife Value	Flowers are rich in pollen and fruit is attractant for a variety of birds



## ADDITIONAL RECOMMENDATIONS

### LOW IMPACT DEVELOPMENT AND GREEN STORMWATER INFRASTRUCTURE

The term low impact development (LID) refers to systems and practices that use or mimic natural processes that result in the infiltration, evapotranspiration or use of stormwater in order to protect water quality and associated aquatic habitat with the use of Green Stormwater Infrastructure (GSI).

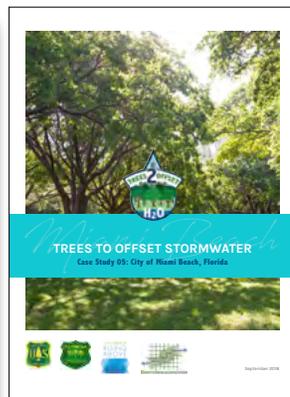
Developers and project engineers are encouraged to first define the project to be consistent with LID, then plan the entire site, and design the stormwater management system to incorporate LID principles and practices as much as possible. Helpful links can be found at the bottom of this page.

Funding Opportunities:

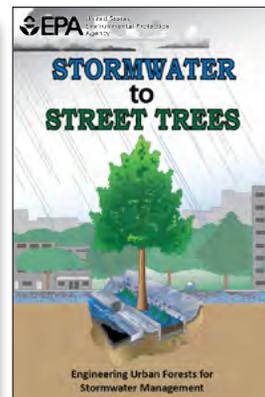
<https://www.epa.gov/green-infrastructure/green-infrastructure-funding-opportunities>



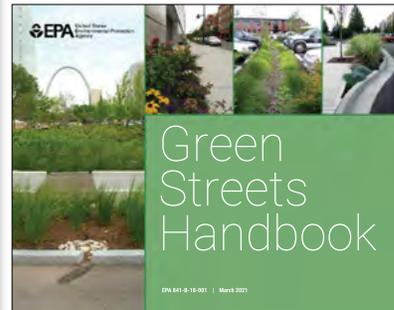
[https://www.epa.gov/final\\_stormwater\\_trees\\_.pdf](https://www.epa.gov/final_stormwater_trees_.pdf)



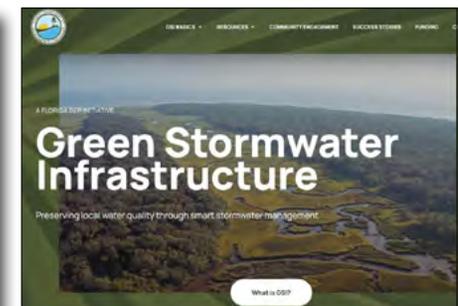
[http://www.gicinc.org/PDFs/1.13\\_MiamiBeach\\_webspread\\_lores.pdf](http://www.gicinc.org/PDFs/1.13_MiamiBeach_webspread_lores.pdf)



<https://www.epa.gov/stormwater2streettrees.pdf>



<https://www.epa.gov>

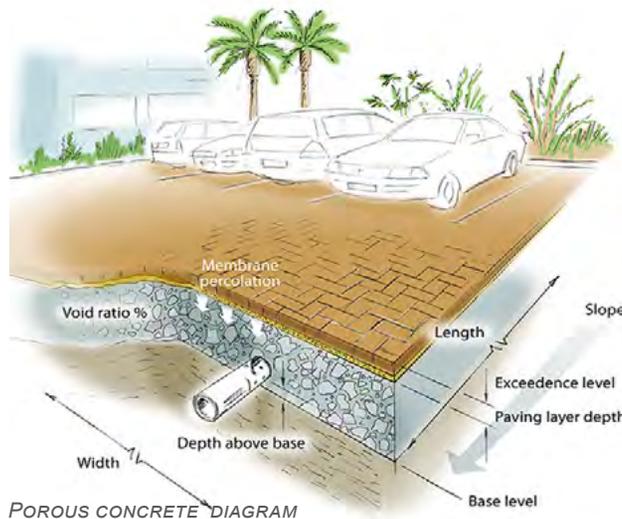


<https://gsi.floridadep.gov/>



ADDITIONAL RECOMMENDATIONS

LOW IMPACT DEVELOPMENT AND GREEN STORMWATER INFRASTRUCTURE



TECHNOLOGIES AND TECHNIQUES

As discussed earlier the Lantana downtown is vulnerable to standing water and flooding. Any downtown improvements should include the reduction of impervious surfaces and the increase in pervious areas for water percolation.

The images at left reflect just a sampling of the effective technologies and techniques for providing pervious parking in the downtown.



ADDITIONAL RECOMMENDATIONS

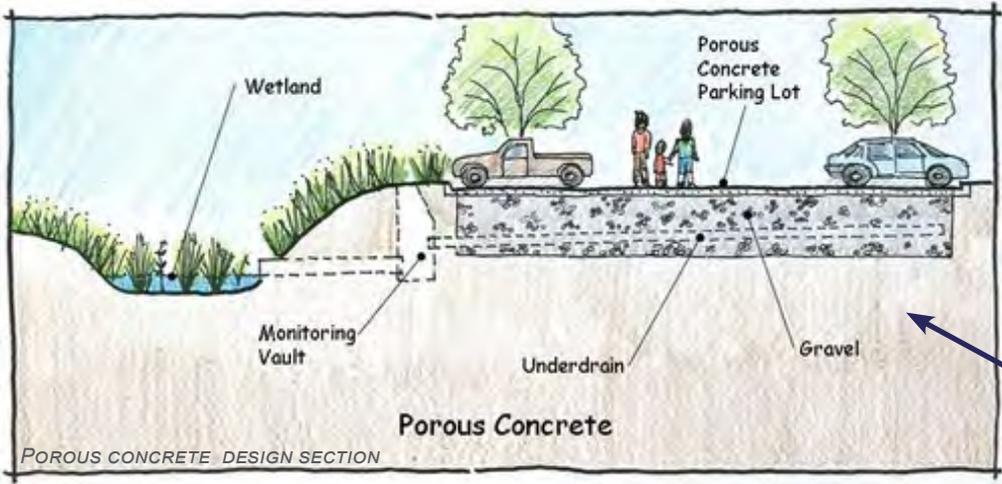
LOW IMPACT DEVELOPMENT AND GREEN STORMWATER INFRASTRUCTURE

TECHNOLOGIES AND TECHNIQUES

There is a strategy in urban planning and development growing in popularity referred to as Low Impact Development.

Low Impact Development is a comprehensive approach to managing stormwater and pollutant reductions within built environments. The goal is to retain and treat stormwater on-site as it occurs as opposed to simply conveying to off-site locations and basins.

In particular, Low Impact Development is a very effective approach to incremental infill redevelopment where no larger municipal stormwater strategies exist or are planned.



LOW IMPACT DEVELOPMENT FOR THE LANTANA BOAT RAMP SITE



RAIN GARDEN



RAIN GARDEN BULB-OUT



### III. KEY RECOMMENDATIONS

	<b>PRIORITIZED ACTION ITEMS</b>	<b>ENTITIES FOR COORDINATION</b>	<b>TIME FRAME</b>	<b>DEPARTMENTS</b>
<b>A</b>	Adopt the Master Plan - Start Public Information Campaign	Town of Lantana	6 Months	Council
<b>B</b>	Develop the Town Center Redevelopment Overlay	Town of Lantana - FDOT	6-12 Months	Planning, Admin., Council
<b>C</b>	Develop the US-1 Corridor Infill Overlay	Town of Lantana - FDOT	6-12 Months	Planning, Admin., Council
<b>D</b>	Revise and Update the Downtown Overlay Zone	Town of Lantana - PBC	12 Months	Planning, Admin., Council
<b>E</b>	Solicit Input on Lantana Beach Master Plan	Town of Lantana - PBC	12-14 Months	Planning, Admin., Council, Parks and Rec.
<b>F</b>	Develop Pedestrian/Transit Improvements (US-1 connections to transit and completing the overall Town-wide sidewalk network)	Town of Lantana - FDOT - Palm Beach TPA - FEC	12-16 Months	Planning, Admin., Council, Parks and Rec., Eng., Legal
<b>G</b>	Pursue Municipal Parking Lot Locations	Town of Lantana	12-16 Months	Planning, Admin., Council
<b>H</b>	Continue Contact with Downtown Property Owners	Town of Lantana	Ongoing	Planning, Admin., Council
<b>I</b>	Update Zoning Code with Priority Items	Town of Lantana	12-16 Months	Planning, Admin., Council, Legal
<b>J</b>	Revise Landscape Code - Modernize	Town of Lantana	16 Months	Planning, Admin., Council, Legal
<b>K</b>	Develop Detailed Plans for On-Street Parking on Oak Street and Lake Dr.	Town of Lantana - PBC	16 Months	Planning, Admin., Council, Public Works, Engineering



### III. KEY RECOMMENDATIONS

	PRIORITIZED ACTION ITEMS	ENTITIES FOR COORDINATION	TIME FRAME	DEPARTMENTS
L	Solicit Input on Municipal Campus Master Plan	Town of Lantana	18-24 Months	Planning, Admin., Council, Legal
M	Solicit Master Plan and Designs for Entry Monuments	Town of Lantana - FDOT - PBC - FEC - FPL	18-24 Months	Planning, Admin., Council, Legal, Public Works, Engineering
N	Consider Historic Preservation Program and Ordinance for the Town of Lantana	Town of Lantana - State of Florida	24-30 Months	Planning, Admin., Council, Legal



APPENDIX A  
CREATION OF THE PLAN



## OUTREACH

Public outreach and participation was an essential ingredient in the creation of this plan and recommendations. Many forms of outreach and opportunities were provided so that all who were interested in the process could participate.

### INDIVIDUAL INTERVIEWS

As part of the initial outreach and information reconnaissance for the project, TCRPC conducted over 20 individual interviews with the elected officials, Town staff, and property and business owners. The interviews provided valuable information for the TCRPC team and helped those in the community understand the issues to be addressed and the process that was utilized.

### PUBLIC DESIGN CHARRETTE

A five-day public design charrette was held at the Town Council Chambers from Saturday, July 9th, 2022 to Wednesday, July 13th, 2022. The public workshop was that Saturday and attracted approximately 60 people from the community. The design team worked in the Council Chambers “studio” which was open to the public, where 10-15 people per day visited and gave input. The studio was open from 9:00 am until 9:00 pm



Citizen table at Charrette workshop.



Citizen table at Charrette workshop.



Opening presentation as part of Charrette.





Sunday through Tuesday and 9:00 am to 5:00 pm on Wednesday.

The Saturday public workshop and the open-to-the-public studio environment were immensely helpful in creating continuous and meaningful dialogue and connection between the design team and the community.

## WORK-IN-PROGRESS

A Work-in-Progress presentation was given the evening of Monday, July 25th, 2022 at the Town Council Chambers and was the first opportunity for the public to see the design work and recommendations and provide their feedback and input.



Since that time the TCRPC team has been refining the recommendations, developing further analysis and diagrams, and compiling the master plan report including the market study findings.

The public outreach will continue after the submittal of this report as there will be additional public presentations and meetings to review and discuss these recommendations.

The following pages include images from the charrette as well as each of the plans developed by the community.



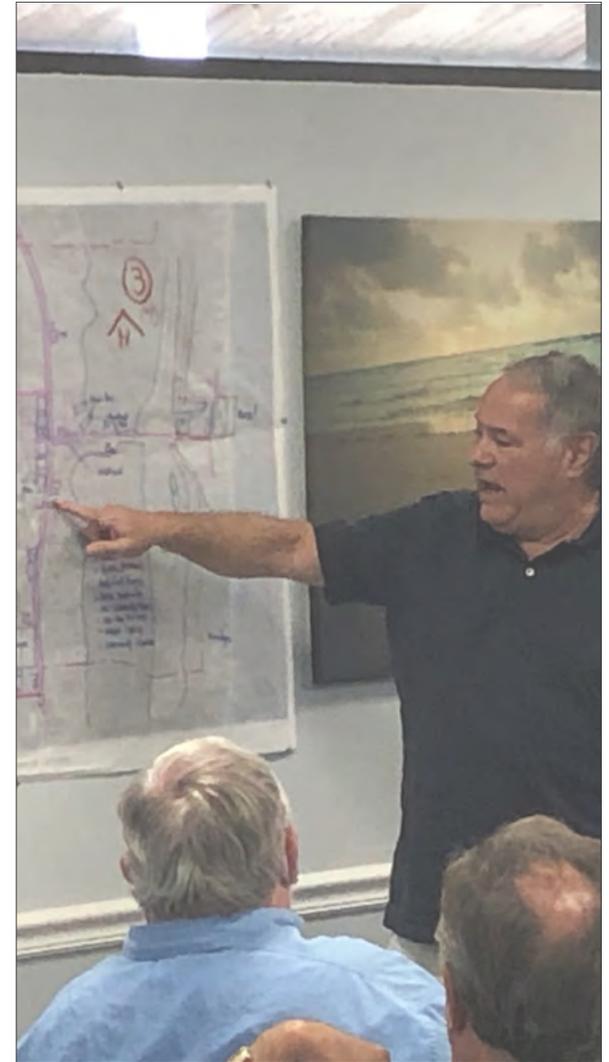
TABLE 1



TABLE 2



TABLE 3



*Citizens presenting their workshop ideas*



TABLE 4

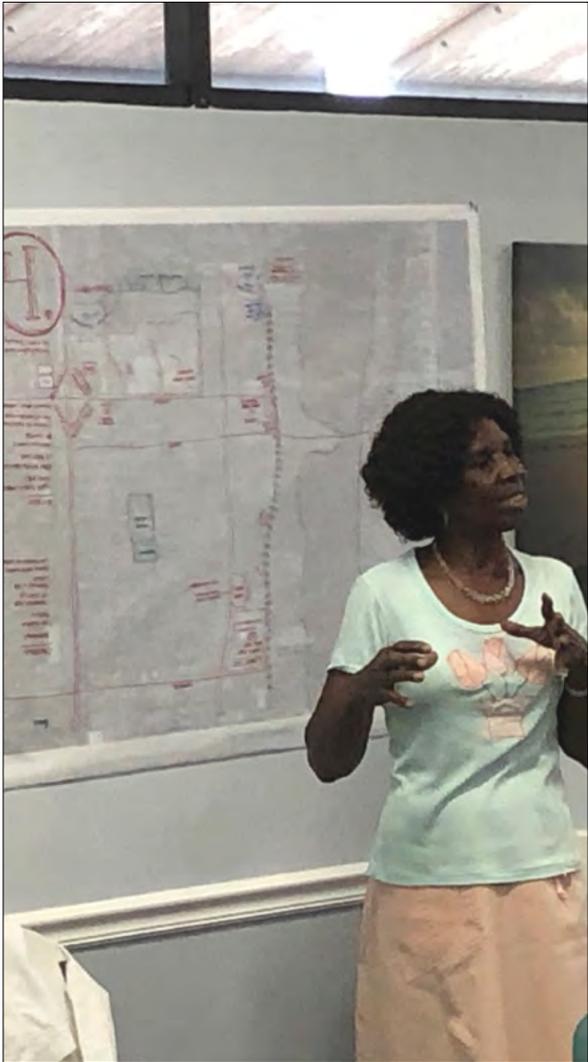


TABLE 5



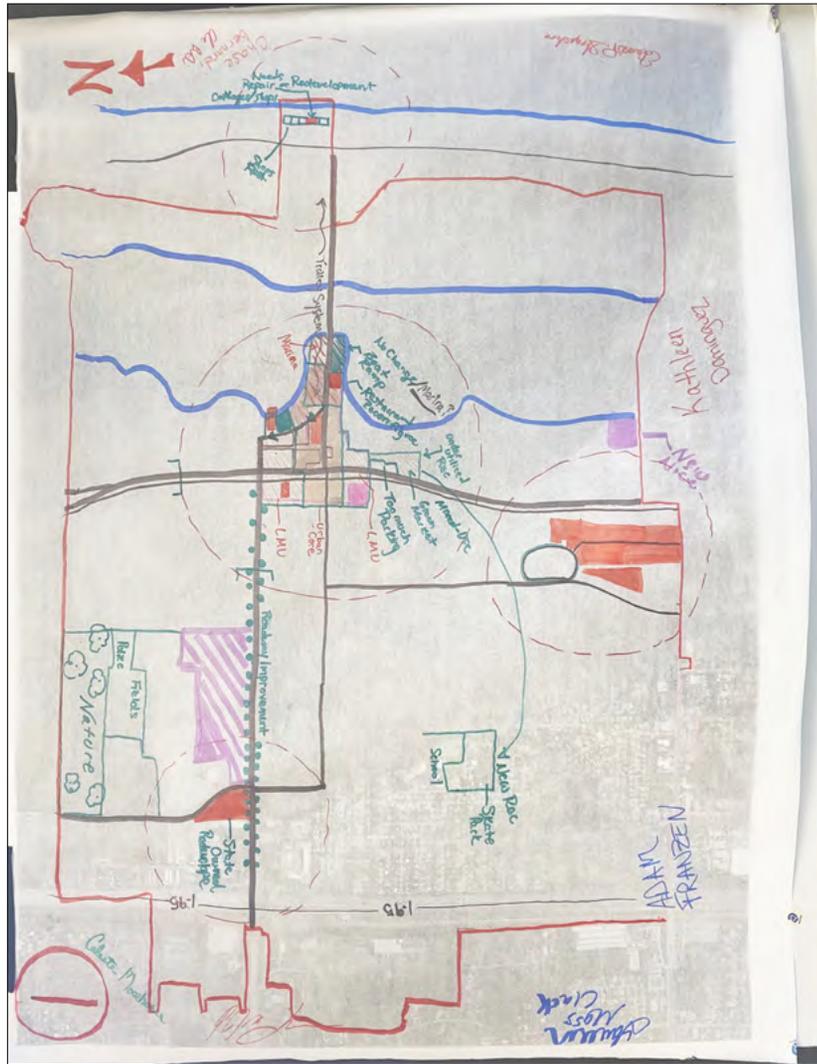
TABLE 6



Citizens presenting their workshop ideas



TABLE 1



Citizen Charrette Plans

TABLE 2

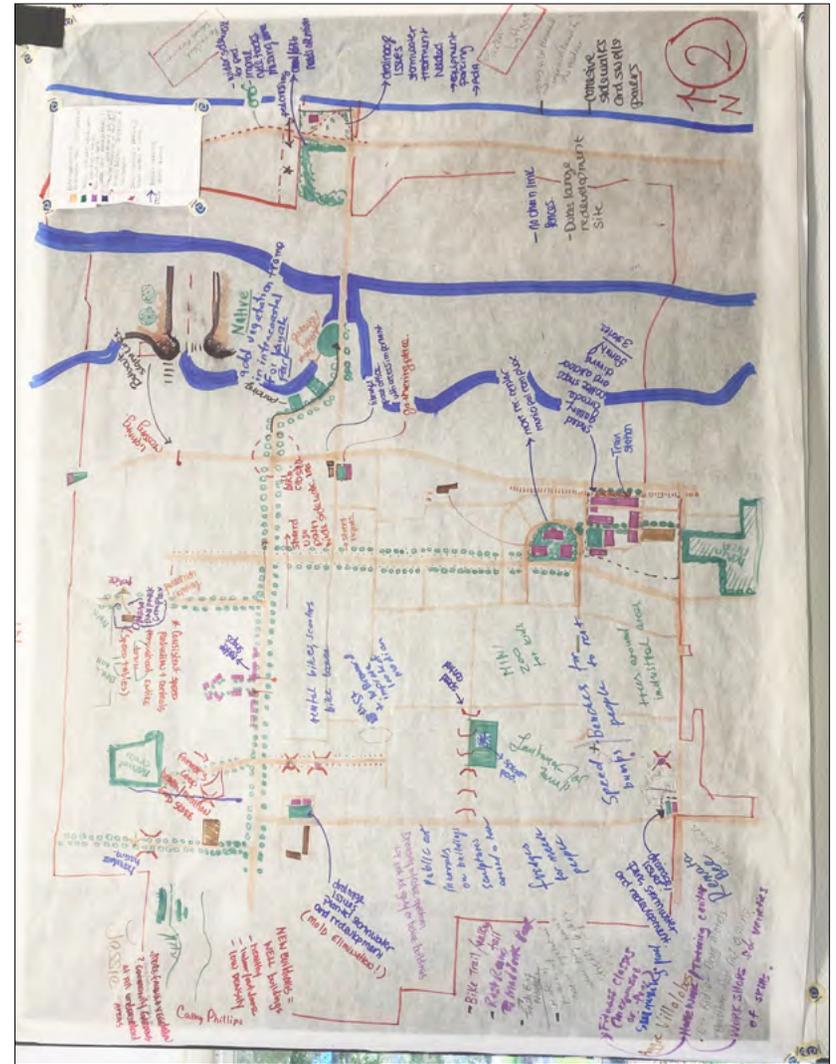
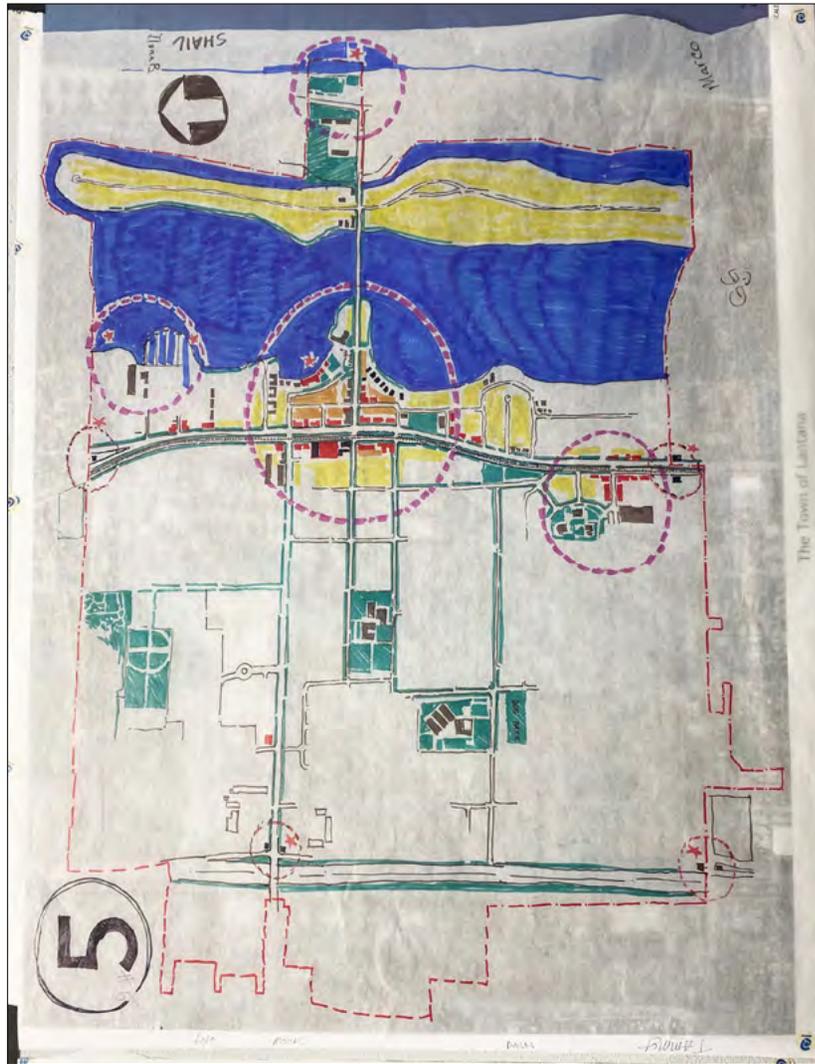


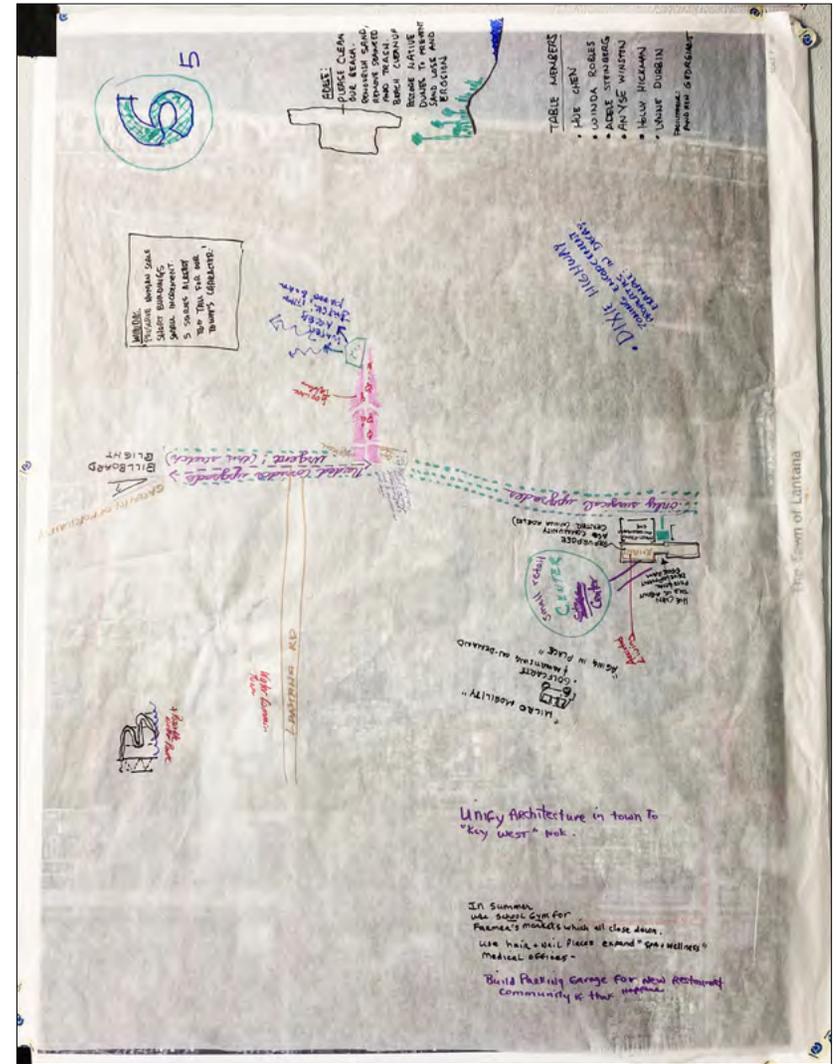


TABLE 5



Citizen Charrette Plans

TABLE 6



# LANTANA CHARRETTE



Town Of  
*Lantana*  
Florida

## Public Workshop Saturday, July 9th

Join the Town and the Treasure Coast Regional Planning Council for a public discussion on the vision for future growth and development in Lantana. Lunch & refreshments will be provided.

- **OPENING PRESENTATION: 10:00AM**
- **PUBLIC DESIGN SESSION: 11:00 AM-1:00PM**  
LUNCH PROVIDED
- **CITIZENS' PRESENTATIONS: 1:30PM-2:30PM**
- **WRAP UP: 3:00 PM**

**LOCATION**

Town Council Chambers  
Town Hall, 500 Greynolds Circle  
Lantana, Florida, 33462  
P: 561-540-5000



**For more information,  
please visit:**  
<https://www.lantana.org/>

## PLEASE PROVIDE YOUR INPUT

The Treasure Coast Regional Planning Council, in collaboration with the Town of Lantana, will conduct a public workshop and design charrette to develop a vision for future growth and development in the Town of Lantana. This effort will result in the creation of conceptual designs, data, and analyses to inform potential future changes to the zoning codes, land uses, and architectural design standards.

- **Saturday Public Workshop**  
Saturday, July 9, 2022: 10:00 am – 3:00 pm
- **Charrette Studio Day #1**  
Sunday, July 10, 2022 9:00 am – 9:00 pm
- **Charrette Studio Day #2**  
Monday, July 11, 2022 9:00 am – 4:00 pm
- **Charrette Studio Day #3**  
Tuesday, July 12, 2022 9:00 am – 9:00 pm
- **Charrette Studio Day #4**  
Wednesday, July 13, 2022 9:00 am – 6:00 pm
- **Work-In-Progress Presentation**  
Monday, July 25th, 2022 6:00 pm – 8:00 pm





The Saturday Public Workshop and Charrette Design Studio (open to the public) will occur in The Town of Lantana Council Chambers.  
Located at 500 Greynolds Circle, Lantana, FL 33462

For more information please contact  
Dana P. Little  
Urban Design Director, Treasure Coast Regional Planning Council  
772.221.4060, [dlittle@tcrcp.org](mailto:dlittle@tcrcp.org)



# APPENDIX B

## BACKGROUND & EXISTING CONDITIONS



## BACKGROUND

This Master Plan report reflects the efforts and collaboration of the Town of Lantana (Town), the Treasure Coast Regional Planning Council (TCRPC), and the residents and business owners of the Town of Lantana. This multi-agency public planning process began in the summer of 2021 when TCRPC was requested to assist in developing a vision for the Town of Lantana.

The Town of Lantana was incorporated in 1921, and at the time only a land area of 1 square mile with a population of 100 residents. The Town has grown to 2.9 square miles and a population of approximately 12,100 today.



*Hypoluxo Island – Early Settlement – The Pierce Estate – “Casa Alva”*



*Emerging Finnish Population in the 1950's Florida Chapter - Finlandia Club est. 1954*

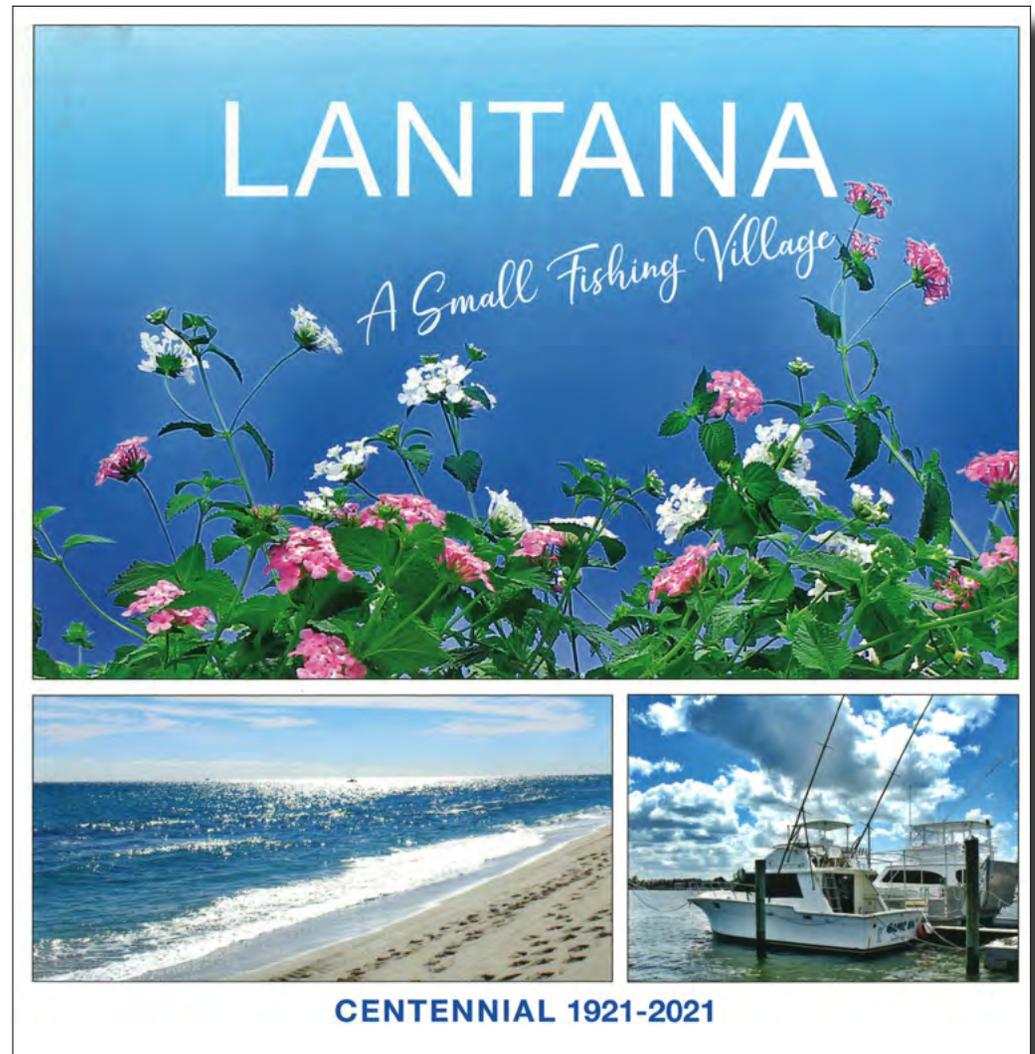


*The “Barefoot Mailmen” 1885-1893 Hypoluxo Island to Miami*



## BACKGROUND

Lantana recently celebrated its Centennial in 2021, and the following images showcase a timeline of the Town over the years from the centennial publication “Lantana A Small Fishing Village”. Images credited from The Town of Lantana Photo Collection and the Historical Society.



BACKGROUND

# Timeline

**1921**

The Town of Lantana was incorporated with 22 people voting in the election. The first two mayors were women: Ellen Anderson and Mary Paddock.

**1923**

AIA was created along the Lantana beach frontage and a small road and bridge connected it to Hypoluxo Island.



Ostrich & Alligator Farm along County Road, now Dixie Highway.

**1925**



The first bridge was built linking Lantana to the beach.



The Ye Tower building was constructed in 1925 as a real estate gimmick by O. Q. Greynolds. Paul Dunbar & his brother Harley operated the restaurant as a hot dog stand which closed on June 17, 1987.

**1926**

Dedicated first post office building.

**1931**

The 40 acres at the southern end of Hypoluxo Island broke away from Lantana to join oceanfront property owners in the incorporation of the town of Manalapan.

**1948**



Initial construction of Finland House (officially named in 1992); an addition was constructed in 1956.

**1950**



The first tuberculosis hospital, A.G. Holley State Hospital opened. Demolished 2014.



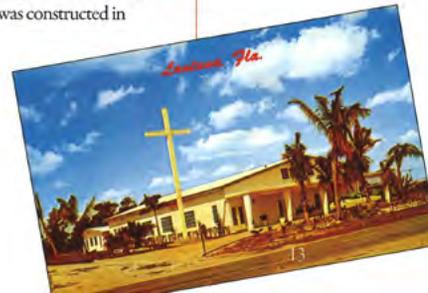
A second draw bridge was opened.



**1953**



Lantana's first Community Library opened by the Woman's Club in the original Bridge Tenders House.



BACKGROUND

**1956**

Anne E'Del Deacon was appointed municipal judge for the Town of Lantana. She was the first female judge in Palm Beach County and fourth in Florida.

**1965-2003**

Lantana was home to Lantana Boatyard Inc, where sleek million-dollar patrol boats were built.

**1971**

National Enquirer moved its world headquarters from New York City to Lantana. The paper moved to Boca Raton in 2000.

**1971**



Lantana was home to the tradition of hosting the largest Christmas tree in the world. Every year a huge tree would be shipped from the Pacific Northwest to Lantana by rail to the grounds of the National Enquirer.

**1976**

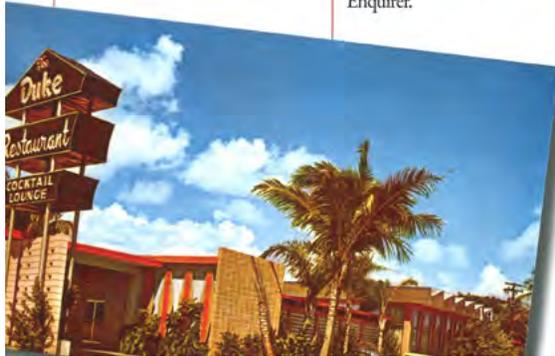


President Gerald Ford visited Lantana Bicentennial Park where he was greeted by Mayor Charles W. Potter, and Ken Sequist, who designed Bicentennial Park.

**1986**



First Annual Christmas Parade.



Town received the first Government Finance Officers Budget Award.

**1989**

Town-wide expansion for recycling program.

**1994**



Improvements to Bicentennial and Sportsman's Parks for approximately \$1 million to include boat ramp refurbishing, parking lot resurfacing, seawall restoration, landscaping improvements and installation of a boardwalk and nature trail.

Purchase of the Carteret Bank as a future site for a Town Library.

**1995**



Opening of new Public Library.

Implementation of the Lantana Library Foundation which was instrumental in bringing dozens of new children's books to the shelves, providing for internet service and maintaining fundraising efforts to support the library to include assisting in funding the renovations in 2020/2021.

**1997**

Development of the Nature Preserve, a 6 1/2 -acre site with walking trails, wildlife, and native flora.

Grand Opening Celebration of the new Sports Complex at 1251 West Lantana Road.

**2007**

Bicentennial Park was renamed as Gerald R. Ford Bicentennial Park.

**2009**

Town residents and supporters save the Lantana Post Office with the "Barefoot Mailman Campaign".



**2012**



Evergreen Cemetery was listed on the Florida Heritage Site.



BACKGROUND



Purchase of 1-acre parcel that is now Lyman Kayak Park.  
Official closing of A.G. Holley Tuberculous Hospital – the last one of two TB hospitals in the Country.

2013



New East Ocean Avenue bridge opened.

2016



Opening of the relocated ball fields to Lantana Sports Park behind the Police Station.

2017

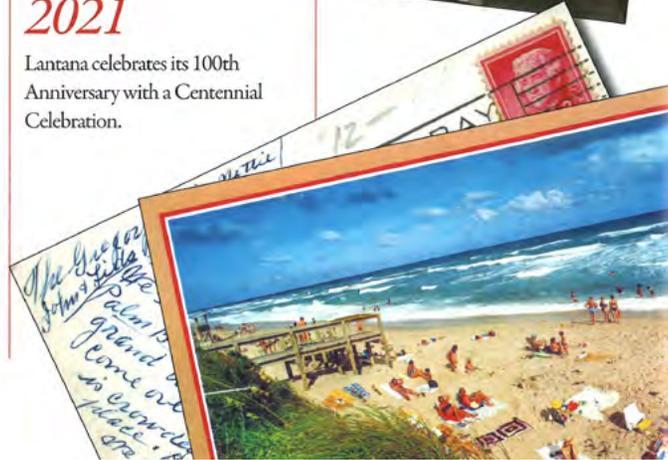
Chinese President Xi Jinping stayed at the Eau Palm Beach, abutting the Town's beach; roads were closed along East Ocean Avenue. with large crowds showing support and opposition of his arrival.

2018

Federal Railroad Administration approved a Quiet Zone regarding the use of locomotive train horns along the FEC corridor at the Town's 5 grade crossings.

2021

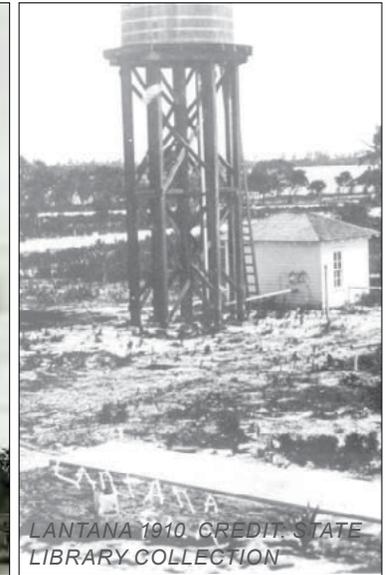
Lantana celebrates its 100th Anniversary with a Centennial Celebration.



LANTANA'S MUNICIPAL BEACH IN THE 1940'S



CHRISTMAS TREE 1970'S



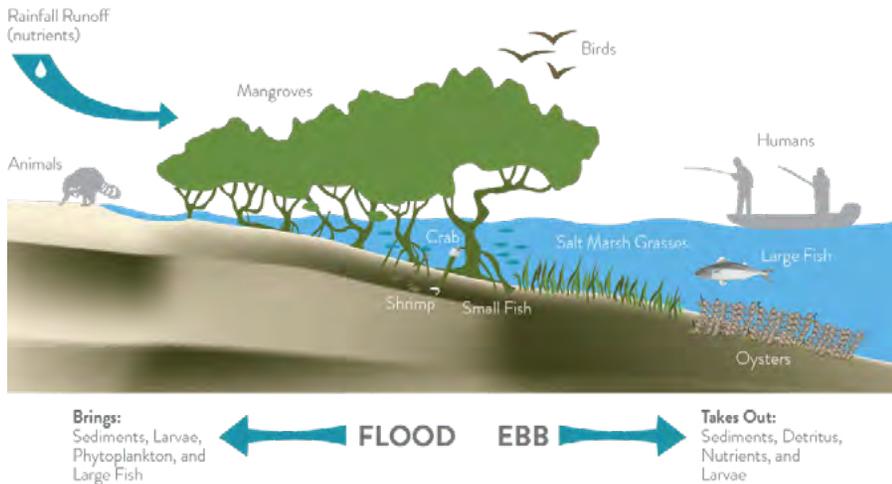
LANTANA 1910. CREDIT: STATE LIBRARY COLLECTION



EXISTING CONDITIONS

The Town of Lantana “A Small Fishing Village” hosts 11 parks and recreation facilities which range from nature preserves that encourage bird watching and hiking to enjoying numerous waterfront amenities like Lantana’s Municipal Beach which has 750’ of beach frontage for swimming.

The Town has a fishing pier for recreational fishing in the Intracoastal waterway and provides 27 boat trailer parking spaces and 37 vehicular parking spaces. The Lyman Kayak Park gives access to portage and proximity to the living shorelines project in the Lake Worth Lagoon.



Living Shorelines infographic



**Town of Lantana**  
**Lyman Kayak Park**  
106 N. Lake Drive  
Lantana, FL 33462





**Hours of Operation:** The Park opens at sunrise and closes at sundown. The parking lot is open 24 hours 7 days a week.

**Amenities:** Passive Park, kayak launch and non-fishing observation dock.








**Town of Lantana**  
**Fishing Pier**  
320 E. Ocean Avenue  
Lantana, FL 33462



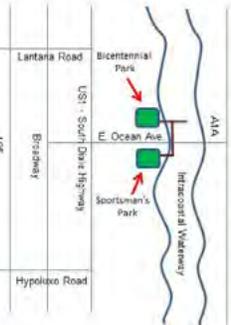


**Hours of Operation:** The fishing pier is located in Sportsman's Park in downtown Lantana. It spans Sportsman's Park with Bicentennial Park. It is open 24 hours a day, 7 days a week.

**Recreation:** The pier is for recreational fishing in the intracoastal waterway. There are three fish cleaning stations throughout the park.

**Parking:** There are 27 boat trailer parking spaces and 37 automobile spaces. A parking decal is required for trailer parking. To obtain a parking decal contact the town at (561) 540-5008.

**NO ALCOHOLIC BEVERAGES ARE ALLOWED IN THE PARK**






Maps and images of waterfront parks in the Town of Lantana



## CURRENT REDEVELOPMENT

Water Tower Commons is a current 73 acre redevelopment effort located along Lantana Road near the western end of the community. It includes a total of 708 recently completed multi-family residential units, and retail space which is under construction including Aldi, WaWa, and a Chick-fil-A. There are also remaining retail sites with frontage on Lantana Road as well as an 83,113 SF community center in the middle of the site.



*Image of Water Tower Commons, a current redevelopment effort.*



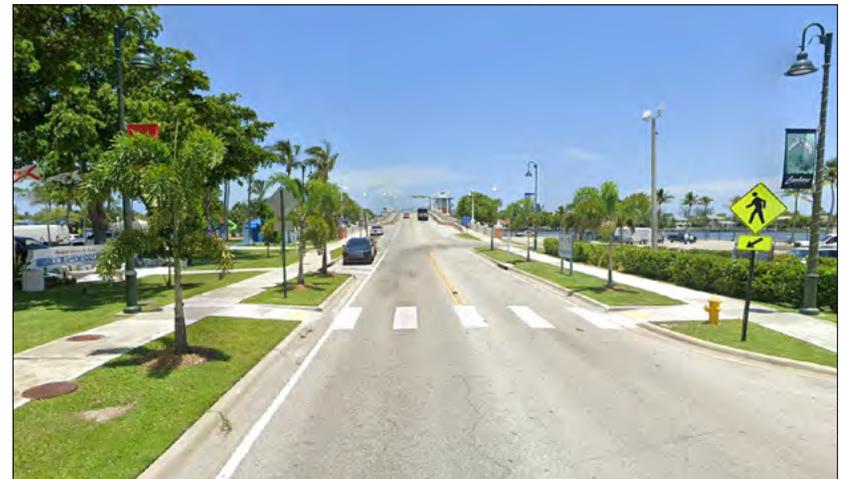
## OCEAN AVENUE EXISTING CONDITIONS

East Ocean Avenue is the historic downtown of Lantana. A quaint street of shops, restaurants, and residential units, East Ocean Avenue is also a regional draw for the famous Old Key Lime House restaurant and dock.

East Ocean Avenue is also the connection to Hypoluxo Island and Lantana Beach which boasts its own regionally renowned restaurant Dune Deck Cafe.



*The Old Key Lime House built in 1889 is a landmark of the Town*



*Views looking East along Ocean Avenue*



**APPENDIX C**  
ECONOMIC MARKET STUDY  
EXECUTIVE SUMMARY

